



North Carolina Department of Cultural Resources  
State Historic Preservation Office

Ramona M. Bartos, Administrator

Pat McCrory, Governor  
Susan Kluttz, Secretary  
Kevin Cherry, Deputy Secretary

Office of Archives and History  
Division of Historical Resources

August 5, 2013

Suzanne Unger Young  
Michael Baker Engineering, Inc.  
8000 Regency Parkway, Suite 600  
Cary, NC 27518

Re: West Street Extension Project, Historic Architectural Resources Survey, Raleigh, Wake County,  
CH 12-1829

Dear Ms. Young:

Thank you for your letter dated June 24, 2013, transmitting the above survey report prepared by Dovetail Cultural Resource Group.

For the purpose of compliance with Section 106 of the National Historic Preservation Act, we concur with the report's findings that the **U.S. Post Office, Morgan Street Station** and the **Capital Coca-Cola Bottling Company Garage** are each eligible for listing in the National Register of Historic Places as contributing resources of an expanded Depot Historic District. The proposed expansion of the district's National Register boundaries appears appropriate.

As you know, we previously concurred that the Boylan Heights Historic District and the existing Depot Historic District (including the expansion area generally between Harrington Street and the railroad tracks) remain eligible for listing in the National Register.

We also concur, that barring additional information to the contrary, that **Woodard's Gulf Service Station** is *not* eligible for listing in the National Register due to alterations to the site (most notably the addition of the canopy) and a loss of its historic integrity. The remaining properties within the project's area of potential effect are also *not* eligible for listing in the National Register.

The above comments are made pursuant to Section 106 of the National Historic Preservation Act and the Advisory Council on Historic Preservation's Regulations for Compliance with Section 106 codified at 36 CFR Part 800.

Thank you for your cooperation and consideration. If you have questions concerning the above comment, contact Renee Gledhill-Earley, environmental review coordinator, at 919-807-6579 or renee.gledhill-

earley@ncdcr.gov. In all future communication concerning this project, please cite the above referenced tracking number.

Sincerely,



Ramona M. Bartos

cc: Kerri Barile, Dovetail Cultural Resource Group [kbarile@dovetailcrg.com](mailto:kbarile@dovetailcrg.com)  
Mary Pope Furr, NCDOT [mfurr@ncdot.gov](mailto:mfurr@ncdot.gov)  
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# Baker

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## LETTER OF TRANSMITTAL



**TO:** Renee Gledhill-Early  
Environmental Review Coordinator  
NC State Historic Preservation Office  
109 East Jones Street, Room 258  
Raleigh, NC 27601

**RE:** Extension of West Street on New Location  
from Cabarrus Street to Martin Street With  
Grade Separation at NC Railroad/Norfolk  
Southern "H" Line, Raleigh, U-5521, Wake  
County, CH 12-1829  
Review of Architectural Studies Report

**FROM:** Suzanne Unger Young, Project Manager

**DATE:** June 24, 2013      **JOB NO.** 129508

*CH 12-1829*

**We are sending via:**     Overnight     Regular Mail     Pick-up     Hand Delivered

**The following items:**     Correspondence     Plans     Specifications     Other as listed below:

Enclosed for your review is a hard copy of the Architectural Studies report and associated materials prepared by Dovetail Cultural Resource Group for the City of Raleigh's proposed Extension of West Street on New Location from Cabarrus Street to Martin Street With Grade Separation at NC Railroad/Norfolk Southern "H" Line, Raleigh, U-5521, Wake County, CH 12-1829.

### THESE ARE TRANSMITTED as checked below:

*See letter for B/S/12*

For Approval     As Requested     Approved As Submitted     Returned For Corrections  
 For Your Use     For Review And Comment     Approved As Noted     Forward To Subcontractor

### REMARKS:

*Date 7/23/13*

Renee,

Attached for your review and comment is a hard copy of the Architectural Studies report prepared by Dovetail Cultural Resource Group (Dovetail) for the City of Raleigh's proposed Extension of West Street on New Location from Cabarrus Street to Martin Street With Grade Separation at NC Railroad/Norfolk Southern "H" Line, Raleigh, U-5521, Wake County, CH 12-1829. At the back of the report is a manila sleeve with a CD-ROM containing a digital copy of the report, a copy of the survey form database detailing the nine individual resources recorded during the survey, and digital images, maps, and site plans for all of the properties recorded during Dovetail's investigation. Hard copies of the survey forms, maps, site plans, and a contact sheet detailing the digital images stored on the CD-ROM are also included at the back of the report.

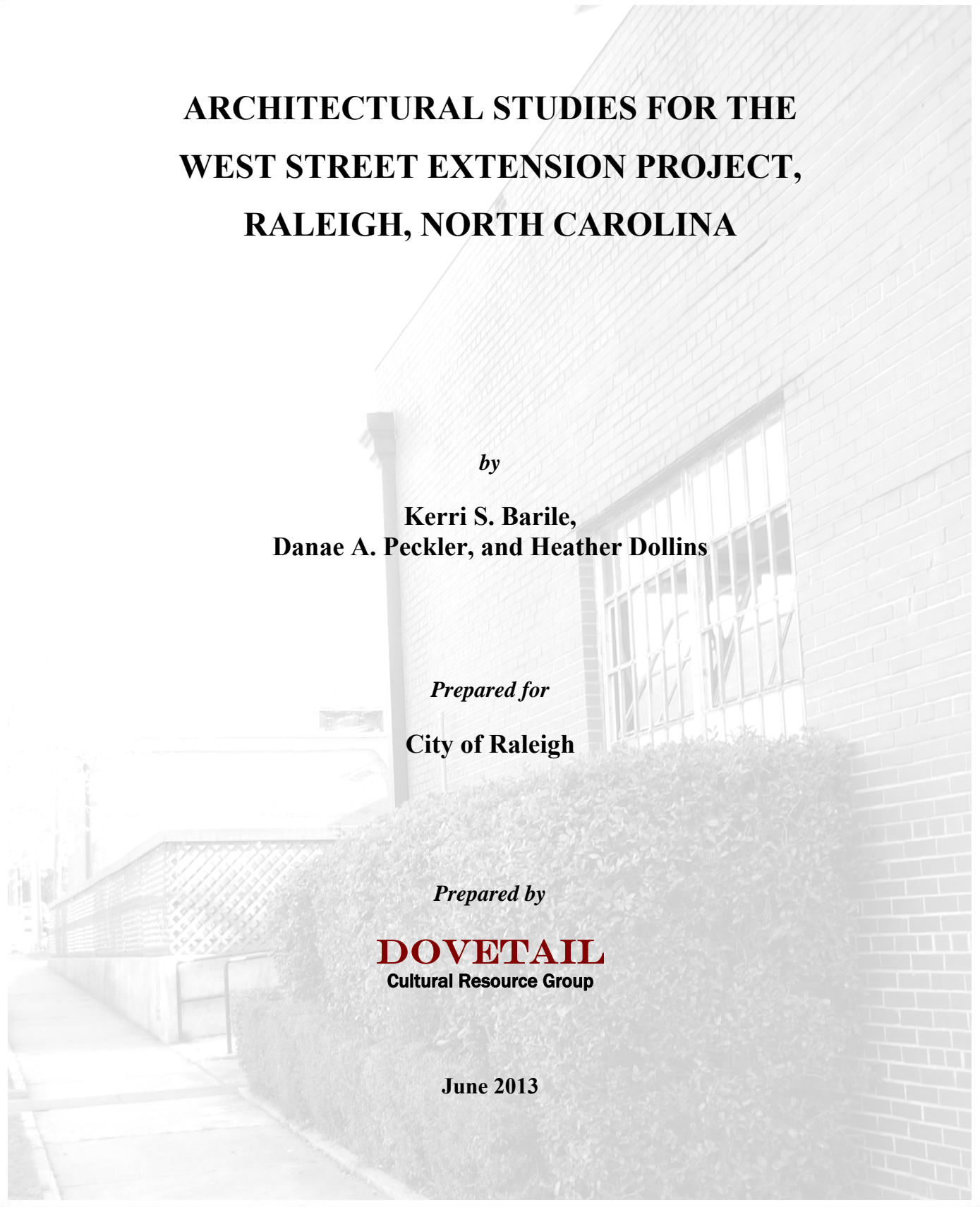
If possible, we ask you to send comments to Eric Lamb at the City of Raleigh ([Eric.Lamb@raleighnc.gov](mailto:Eric.Lamb@raleighnc.gov)) within 30 calendar days of your receipt of this package. If you have questions, please feel free to contact me at 919-481-5716 or Eric at 919-996-2161.

Sincerely,

*Suzanne Unger Young*  
Suzanne Unger Young  
Project Manager

cc: Eric Lamb, City of Raleigh





**ARCHITECTURAL STUDIES FOR THE  
WEST STREET EXTENSION PROJECT,  
RALEIGH, NORTH CAROLINA**

*by*

**Kerri S. Barile,  
Danae A. Peckler, and Heather Dollins**

*Prepared for*

**City of Raleigh**

*Prepared by*

**DOVETAIL**  
Cultural Resource Group

June 2013



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**Architectural Studies for the  
West Street Extension Project,  
Raleigh, North Carolina**

*Prepared for*  
**City of Raleigh**

via

**Baker Engineering**  
1598 Mail Service Center  
Raleigh, North Carolina 27699-1598

*Prepared by*

Kerri S. Barile (Principal Investigator),  
Danae A. Peckler, and Heather Dollins

**Dovetail Cultural Resource Group**

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Dovetail Job #12-041

**June 2013**

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## MANAGEMENT SUMMARY

Dovetail Cultural Resource Group I, Inc. (Dovetail), conducted a historic architectural survey within the study corridor for the proposed extension of S. West Street in Raleigh, North Carolina, for Baker Engineering (Baker) on behalf of the City of Raleigh (City). The City, in cooperation with the Federal Railroad Administration (FRA) and the North Carolina Department of Transportation (NCDOT), is conducting project development, engineering, and National Environmental Policy Act (NEPA) studies for the proposed extension of S. West Street across the Boylan railroad wye. At present, S. West Street is truncated on either side of the railroad, preventing any cross traffic. In addition to the need to provide better connectivity for pedestrians and vehicles across the wye, the recent proposed redevelopment of a property within the wye for use as a multimodal transit center necessitates better access to and from the wye area. The City is evaluating two alternatives in the West Street Extension Environmental Assessment (EA): road-over-rail and road-under-rail connecting the two halves of S. West Street. The project would also close the existing at-grade railroad crossing of W. Cabarrus Street.

The project study area encompasses a space that generally extends from S. Boylan Avenue to S. Harrington Street from W. Morgan Street to W. South Street along the Boylan wye. The Area of Potential Effects (APE) for architectural resources includes all areas where direct or indirect alterations to a property’s setting and feeling could occur. Because of potential FRA funding, the undertaking is required to comply with Section 106 of the National Historic Preservation Act of 1966, as amended (36 CFR 800).

The West Street Extension Project is composed of two phases. The first phase included a reconnaissance architectural survey and creation of a building inventory within the project APE. The purpose of this initial investigation was to determine if any historic resources within the APE had potential for listing on the National Register of Historic Places (NRHP). Fieldwork for the reconnaissance survey, conducted in December 2012, identified 12 architectural properties over 50 years in age. In a letter of March 18, 2013, the North Carolina Historic Preservation Office (HPO) determined 10 of the 12 resources to be not eligible for the NRHP. Two properties were selected for further study, one of which has since been identified as two separate resources. Therefore, the second phase of work included three intensive-level surveys, as recommended by the HPO. The two adjacent resources were evaluated in association with the Depot Historic District (WA-0724). Based on data taken in the field and additional archival research, Dovetail makes the following recommendations regarding the NRHP eligibility of these three properties:

Table 1: Eligibility Recommendations.

| Survey # | Name of Resource/ Address                             | Recommended Evaluation                 |
|----------|---|--|
| 8        | Woodard’s Gulf Station, 520 W. South Street           | Recommended Not Eligible for NRHP      |
| 11(a)    | USPO Morgan Street Branch, 411 W. Morgan Street       | Contributes to Depot Historic District |
| 11(b)    | Coca-Cola Bottling Co. Garage, 117–121 S. West Street | Contributes to Depot Historic District |

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## INTRODUCTION

Dovetail conducted a historic architectural survey within the study corridor of the proposed extension of S. West Street in Raleigh, North Carolina, for Baker on behalf of the City of Raleigh. In cooperation with the FRA and NCDOT, the City is conducting project development, engineering, and NEPA studies for the proposed extension of West Street across the Boylan railroad wye, where three rail lines converge in a triangular shape near the southwest corner of the original city grid. At present, S. West Street is truncated on either side of the railroad, preventing any cross traffic. The purpose of this project is to provide better connectivity for pedestrians and vehicles across the wye, but also to facilitate the proposed redevelopment of a property within the wye for use as a multimodal transit center, thus necessitating safer and more efficient transportation access to this area. The City is evaluating two alternatives in the West Street Extension EA: road-over-rail and road-under-rail connecting the two halves of S. West Street. The project would also close the existing at-grade railroad crossing of W. Cabarrus Street.

The project study area encompasses a space that generally extends east from S. Boylan Avenue to S. Harrington Street and south from W. Morgan Street to W. South Street along the Boylan wye (Figure 1, p. 3). The APE for architectural resources includes all areas where direct or indirect alterations to a property's setting and feeling could occur. Because of FRA funding, the undertaking is required to comply with Section 106 of the National Historic Preservation Act of 1966, as amended (36 CFR 800).

The West Street Extension Project is composed of two phases. The first phase included a reconnaissance architectural survey and creation of a building inventory within the project APE. The purpose of this initial investigation was to determine if any historic resources within the APE had potential for listing on the NRHP. Fieldwork for the reconnaissance survey was conducted in December 2012 and identified 12 architectural properties over 50 years in age. In a letter dated March 18, 2013, the HPO determined 10 of the identified resources to be not eligible for listing on the NRHP. Two resources were selected for further study, one of which has since been identified as two separate properties. Therefore, the second phase of work included three intensive-level surveys as recommended by the HPO: 411 W. Morgan Street, 117–121 S. West Street, and 520 W. South Street. The two adjacent resources, 411 W. Morgan Street and 117–121 S. West Street, were to be evaluated in association with the recently expanded Depot Historic District (WA-0724).

This report details the results of the reconnaissance and intensive-level surveys for this project. Results of the reconnaissance survey and building inventory are detailed in Appendix A, while the results of the intensive investigations and recommendations on the NRHP eligibility of three select resources are presented within the body of this report. To be more specific, Appendix A includes the building inventory table and corresponding maps, a copy of the HPO letter noting properties determined not eligible for the NRHP and those selected



for further study dated March 18, 2013, as well as brief descriptions, photographs, and evaluations of all historic resources identified as a result of this undertaking.

Appendix B contains a list of 38 resources that were located within the boundaries of four historic districts: Boylan Heights Historic District, Depot Historic District, S. Boylan Avenue Historic District, and the Old Fourth Ward Historic District. During the current survey, these properties were recorded as part of the historic district instead of as individual resources during the reconnaissance survey. Furthermore, Dovetail's survey builds upon other cultural resource studies recently conducted in the project area. Therefore, Appendix C contains a list of the architectural properties that were excluded from the current study because they were surveyed and evaluated in another investigation during the past year.

Dovetail staff members Danae A. Peckler, Heather Dollins, Caitlin Oshida, and Dr. Kerri S. Barile conducted the work for this undertaking, with Dr. Barile serving as Principal Investigator. All four individuals meet, or exceed, the standards established for both Architectural Historian and Historian by the Secretary of the Interior (SOI). Work on this project was conducted between November 2012 and June 2013 in accordance with relevant state and federal guidelines as part of the compliance process established in Section 106 of the National Historic Preservation Act of 1966, as amended (36 C.F.R. 800). This report meets the standards set forth by the HPO's Architectural Survey Manual, *Practical Advice for Recording Historic Resources*, as well as those outlined by NCDOT in *Sections 106 Procedures and Report Guidelines*.

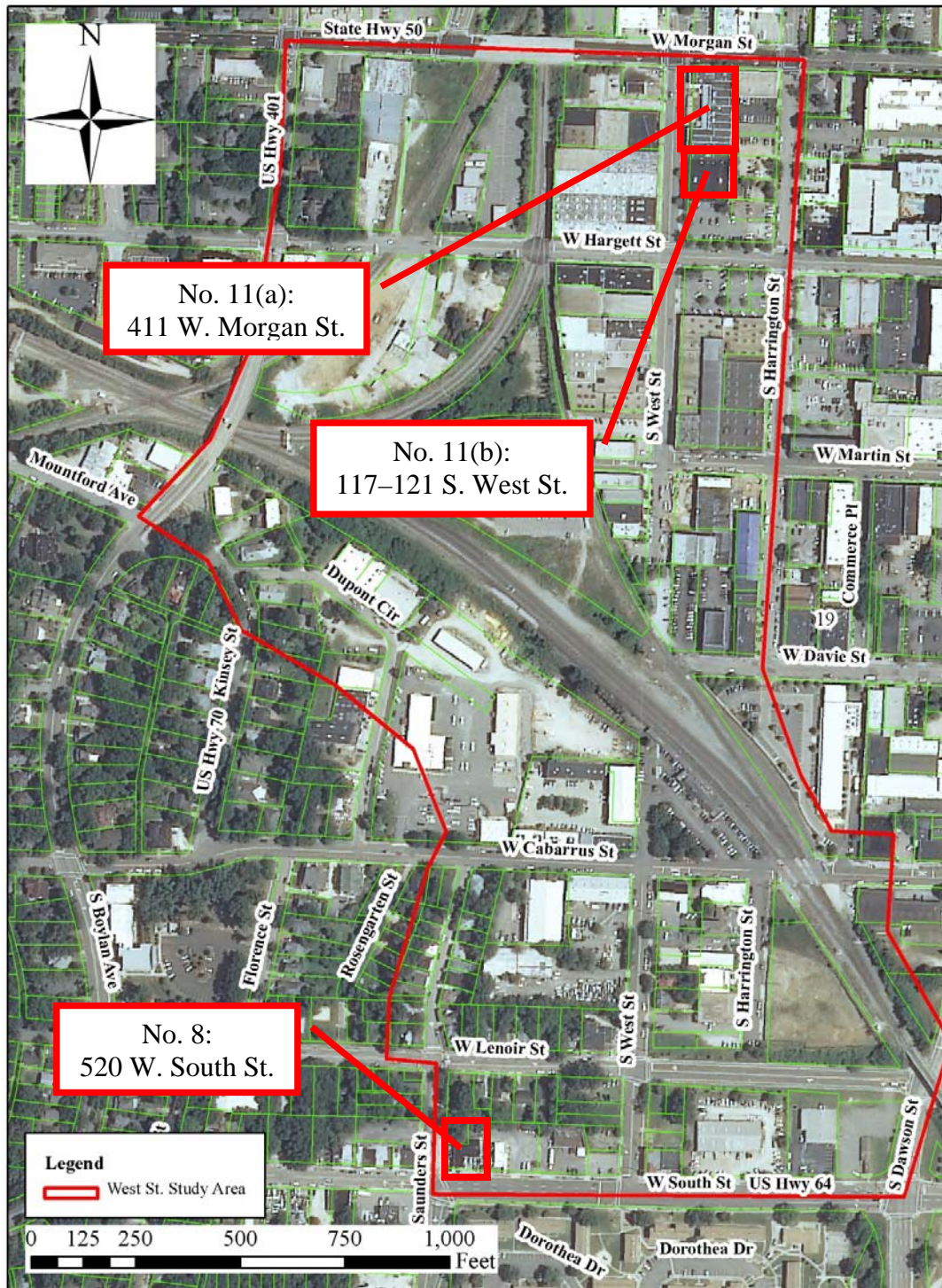


Figure 1: Map of the West Street Extension Project Study Area with Architectural Properties Selected for Further Study Highlighted in Red.

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## PROJECT METHODOLOGY

This project complies with Section 106 standards for the identification of historic properties. The goals of the survey were first, to identify architectural resources over 50 years in age within the project APE—as well as more recently constructed resources of exceptional significance; and second, to document the historic properties through field notes and digital photography. Following this initial documentation and fieldwork, the third goal of the survey was to evaluate each property within the NRHP framework, assessing both historic significance and historic integrity. The fourth and final goal of this work used these significance and integrity assessments to inform recommendations on the NRHP eligibility of each property and present these findings to officials at the HPO.

Following a review of HPO records, Dovetail conducted a reconnaissance survey of the project study area in December 2012. During this survey, Dovetail staff visibly inspected the APE and recorded 100 percent of the existing historic resources; no above-ground resources were found that met the NRHP's additional criteria considerations (A–G). Once identified, each historic resource was documented through photographs, written field notes, and GIS mapping. Digital photographs were taken of each surveyed property's general setting, primary elevation, oblique angles, and significant architectural details. Inventoried resources were assigned a number corresponding to the building inventory and keyed to a map of the study area. The building inventory, corresponding maps, brief descriptions, and photographs of the 12 historic properties recorded in this effort are presented in Appendix A.

HPO staff reviewed the building inventory, and concurred in a letter dated March 18, 2013, that no additional review or evaluation was needed for 10 of the 12 historic resources recorded within the project study area. Three other historic properties were recommended for intensive-level survey along the proposed project corridor: 411 W. Morgan Street, 117 S. West Street, and 520 W. South Street. Goals of the intensive survey were first, to gather additional historic and physical data for these historic properties to document architectural details, construction methodology, and chronology of each resource. The second goal of each intensive survey was to examine the history of each property within the appropriate context. Third, Dovetail sought to make recommendations regarding eligibility under established criteria for the NRHP and reach the fourth goal of the survey—proposing NRHP boundaries for any property that was recommended eligible for listing.

In late-March and early-April 2013, Dovetail visited several repositories in Raleigh to examine records regarding the city's historical development and gather specific information on the history of each selected resource. These repositories included the Olivia Rainey Library, the State Library of North Carolina, more specifically the Government and History Research Room, Photographic Collections, and State Archives. Primary sources like city directories, local newspapers, promotional pamphlets, photographs, and historic maps were examined. Numerous secondary sources detailing Raleigh's history were also consulted, and relevant records at the HPO's Survey and National Register Branch were reviewed. Additional sources were retrieved online, including deeds, plats, and various maps, from

several government sources like the Wake County Register of Deeds, the North Carolina Maps Collection, Library of Congress, and National Park Service websites.

Once archival research was completed, Dovetail conducted an architectural field survey of the selected properties. Each property was visually inspected, recording exterior and interior spaces through written notes and digital photographs. Site plans of each property showing the location of all buildings and landscape elements were created, as well as interior-plan drawings, wherever possible. The historic development, architecture, and cultural significance of each resource were then assessed and evaluated within their respective contexts according to the criteria established for the NRHP. The results of this intensive-level investigation and NRHP evaluation are presented in following chapter of this report.

## RESULTS OF ARCHITECTURAL INVESTIGATIONS

For the purposes of this report, the results of this architectural survey have been divided into two sections. The first contains two properties that were evaluated as potential additions to an existing district: the Depot Historic District (WA-0724). In 2013, the HPO approved an expansion of this NRHP-listed district as proposed by Mattson, Alexander Associates, Inc. (MAA). During this investigation, two resources were further investigated in conjunction with the recent NRHP-eligible boundary expansion of the Depot Historic District: 411 W. Morgan and 117–121 S. West Street. The second section presents an analysis of one property: the gas station at 520 W. South Street that is being recommended not eligible for listing on the NRHP as a result of this study.

### Resources Evaluated as Potential Additions to the Depot Historic District

#### *No. 11: Warehouse, 411 W. Morgan Street and 117 S. West Street*

11(a): United States Post Office (USPO) Morgan Street Station

411 W. Morgan Street  
Raleigh, North Carolina  
PIN: 1703580892



#### *Physical Description*

Formerly the Morgan Street Station of the USPO in Raleigh, the one-story, U-shaped, warehouse located at 411 W. Morgan Street was constructed in 1950 (Photo 1, p. 8). The building originally faced north on W. Morgan Street, with automobile access gained from S. West Street in the center of the west elevation. The continuous foundation of the building is not visible, but its masonry walls are composed of concrete block primarily clad in a five-to-one common-bonded brick veneer. A stretcher-bonded Roman brick veneer acts as a water table along the north elevation and wraps around the northwest corner of the building. The flat roof is surrounded by a parapet wall topped by metal coping. One interior, brick chimney is visible from the southeast corner of the building.

Historically, the primary entrance was centrally located on the north elevation, filled by two sets of double-leaf doors, accessed by the public sidewalk on the south side of W. Morgan Street (Photo 2, p. 8). A single-leaf metal door is located at both the east and west ends of the north elevation. At present, all entries along the north elevation appear to be out of use. Today, entrance to the building is gained from the west elevation off of S. West Street. In



the center of the west elevation is a recessed area that was previously used as a loading space, but is now filled by a wooden deck enclosed with a latticework railing (Photo 3, p. 9). A poured-concrete sidewalk along the north side of the deck leads to a series of single- and double-leaf doors, many of which are found below the shed-roof canopy that covers the old loading bays. The canopy is covered in corrugated metal and supported by riveted metal trusses (Photo 4, p. 9).



Photo 1: View of North Elevation, 411 W. Morgan Street, Looking Southwest.

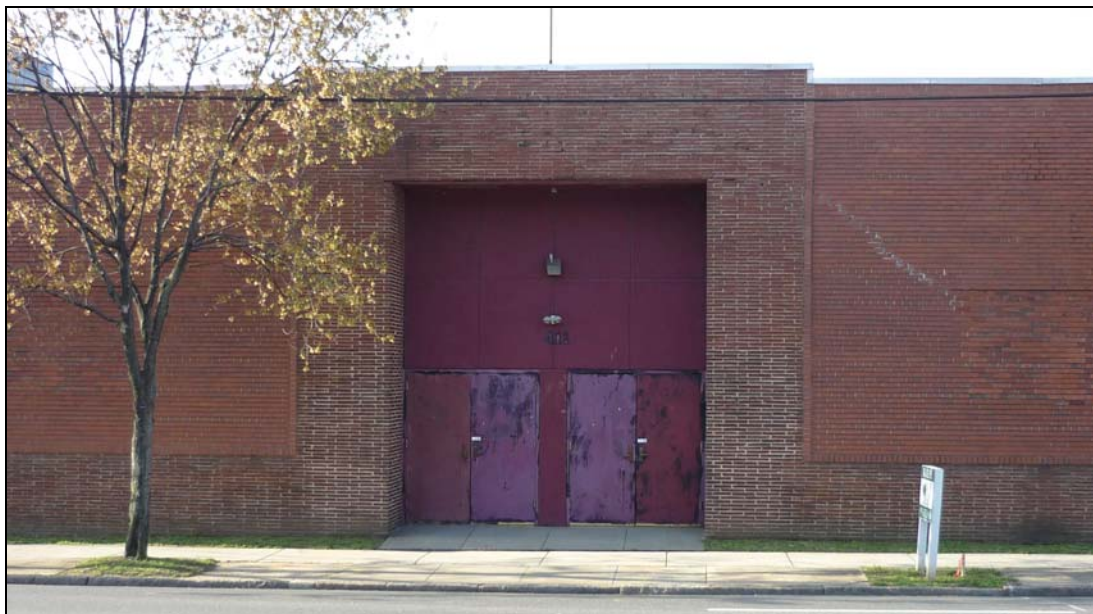


Photo 2: Detail of Historic Entry, Showing Contrast Between Brick Veneers and Other Modifications to the Façade, Morgan Street Elevation.



In use by the USPO until the late 1970s, the west elevation loading area was filled when the building was re-adapted into a cafe and night club in the 1990s. Though historic patterns of fenestration in this area remain, these openings have been filled with brickwork or replacements materials including single-pane, fixed, metal-framed transom windows, and contemporary, double-leaf, metal doors with faux paneling filling the open loading bays. An outdoor bar topped with a tile counter was also constructed under the canopy in recent years (Photo 5, p. 10). The south elevation of the warehouse directly abuts the building addressed as 117 S. West Street and is hidden from view (Photo 6, p. 10).



Photo 3: View of Loading Area, West Elevation, Looking South.

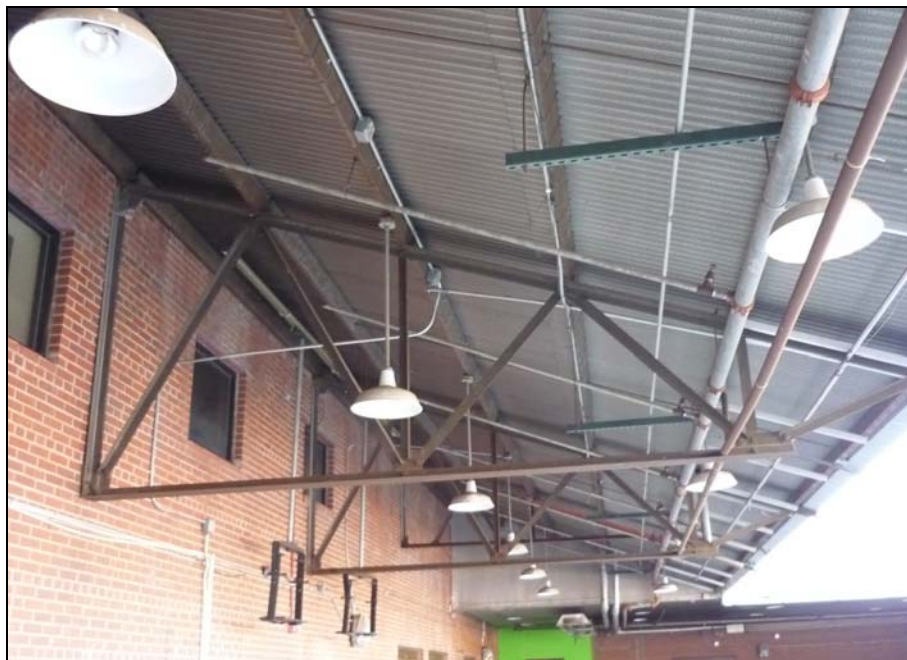


Photo 4: Detail of Canopy and Replacement Windows, West Elevation.



Photo 5: View of Outdoor Bar and Modified Fenestration on West Elevation.



Photo 6: View of West Elevation, Looking North-Northeast. Cold joint in masonry wall and change in parapet height and coping at right denote the separation of 411 W. Morgan Street from 117-121 S. West Street (highlighted by red arrows).

Much of the east elevation of building appears unaltered since construction. Originally, this side of the building appears to have contained 12 bays, each filled with paired, multi-light, steel-framed windows and brick sills (Photo 7, p. 11). Each window is filled with what is likely the original “safety” glass, and contains a central hopper sash (Photo 8, p. 11). Construction of the neighboring buildings at 100 S. Harrington Street was also led by John W. Coffey & Son, and does not appear to have changed the overall design of the USPO building. One bay near the southern end of the east elevation has been bricked-in, but appears to have originally been open for use by automobiles or trucks (Photo 9, p. 11)





Photo 7: East Elevation Showing Original Windows. Lean-to additions and buildings at left and right are not part of the study property.



Photo 8: Detail of Original Windows Along East Elevation.



Photo 9: View of East Elevation Looking Northwest, Showing Chimney. One-story gabled building attached to the east elevation (at right) is a part of another property.

The interior of the building at 411 W. Morgan Street appeared vacant was not accessed at the time of survey.

The setting of this resource remains much the same since it was constructed in the mid-twentieth century, but recent demolitions and new construction appear to be underway on the west side of S. West Street on the same block (Figure 2). Some paved and gravel parking lots are located to the east, or rear, of the building. At the north and west sides of the building concrete sidewalks and small strips of grassy median with several street trees and well-groomed shrubbery line adjacent roadways.

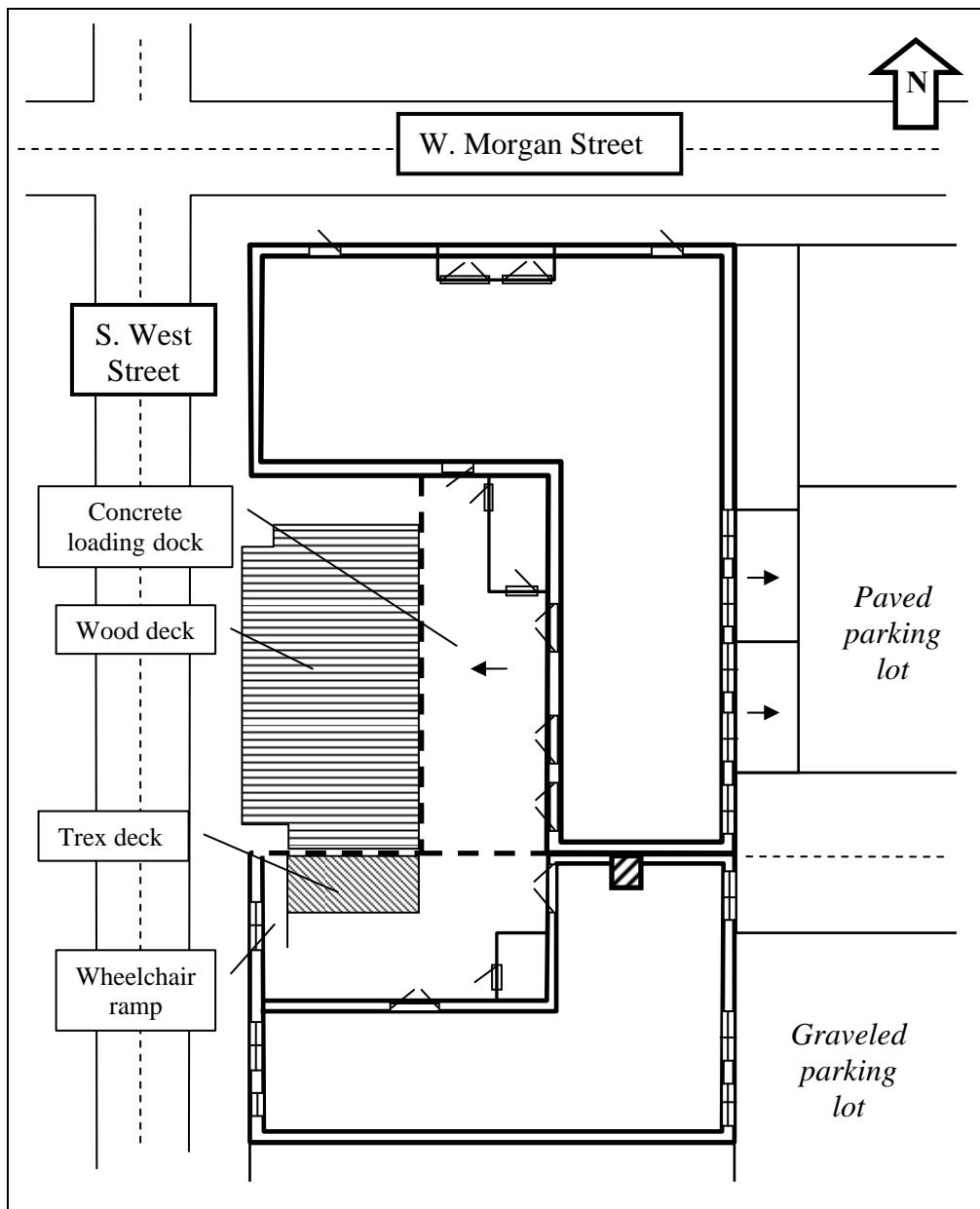


Figure 2: Site Plan, 411 W. Morgan Street. Not to scale.

Some modifications have been made to the building's façade and west elevation over time. This includes the infill of original openings and some replacement materials, as well as alterations to the historic points of entry. However, the massing and overall design of the warehouse appears to be relatively intact. Though integrity of location, setting, and design is moderately high, the resource has diminished integrity of materials, workmanship, feeling, and association.

### *History*

The lot of land on which the warehouse at 411 W. Morgan Street was constructed shortly after it was purchased in February 1950 by John N. and Thelma G. Coffey from L. Conway and Louise B. Murchinson (Wake County Deed Book [WCDB] 1038:413). For 25 years, Murchinson had operated Hotel Wiley at this location, making use of the circa-1900 Wiley Graded Elementary School building to suit guest rooms, a barber shop, and space for a merchant in the basement (Figure 3). The Wiley School was a remnant of the area's history as a residential neighborhood, but was replaced by 1924 with a masonry building near St. Marys (North Carolina Archives [NCA], Photographic Collection; Raleigh City Directory [RCD] 1924). The two-story frame building that initially housed the Wiley School is visible on the 1914 Sanborn Fire Insurance Map, while its replacement, the USPO Morgan Street Station building, is visible on the updated map dating from 1950 (Figure 4, p. 14). The circa-1950 building was identified in the 1950 Sanborn as the "Morgan Street Branch" of the USPO with "brick-faced cinder block, steel beams, and a concrete floor." This Sanborn map also observed a 15-inch parapet along W. Morgan Street, while a 12-inch parapet marked the east, west, and southern sides of the structure.



Figure 3: Early-Twentieth-Century Photograph of Wiley School, Later Wiley Hotel, on Site of USPO Morgan Street Station (NCA, Photographic Collections).

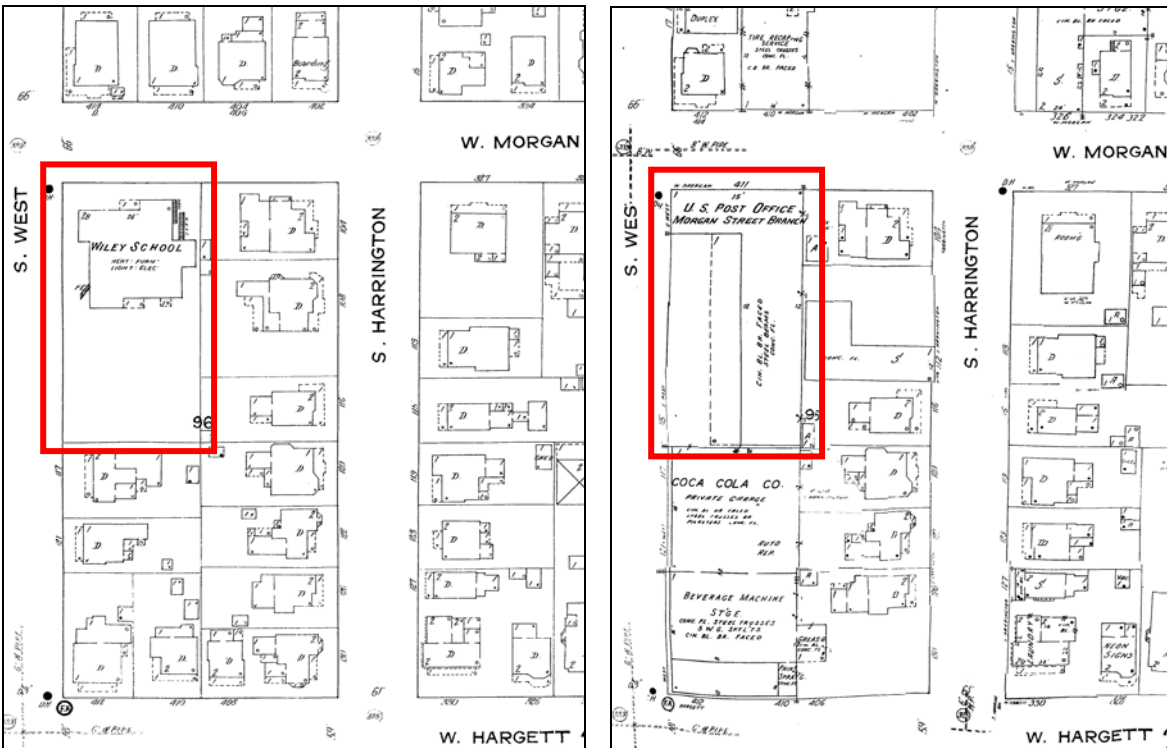


Figure 4: At Left, Detail of Sheet 12, 1914 Raleigh Sanborn Fire Insurance Map and at Right, Detail of Sheet 12, 1950 (Proquest 2001). Property of study highlighted in red.

President of John Coffey & Son General Contractors, John Nelson Coffey appears to have constructed the building at 411 W. Morgan Street with the spatial and architectural needs of the USPO in mind. In November 1950, John and Thelma Coffey entered into a lease agreement with the federal government, renting the newly constructed building for use as a post office and garage known as Morgan Street Station at the rate of \$20,000 per annum for a minimum of 10 years (WCDB 1059:191, 1061:116). John N. Coffey was a native of Raleigh, a graduate of UNC Chapel Hill, and sat on the City Council for a time in the late 1950s, serving as chair of the public works committee.

In 1960, Coffey acquired the lot just south of the USPO warehouse from the Capitol Coca-Cola Bottling Company containing a portion of their garage and beverage machine storage space (WCDB 1408:316). It is believed that the USPO expanded its operations in the area around the same time. The existing L-shaped building visible in the 1950 Sanborn map differs from the U-shaped building seen in the 1959 historic aerial (Figure 5, p. 15). The 1962 City directory notes the USPO's occupation of the space at 121 S. West Street, observed to be in use as the USPO garage. However, these buildings were separated by 117 S. West Street, filled by Midtown Roller Rink at that time and by the Wake County Sheltered Workshop and Vocational Training Center several years later (RCD 1962; 1967).

The Morgan Street Station building was occupied by the USPO until 1970. John and Thelma Coffey had leased the property to the Fairmont Foods Company in 1972 when they sold the



lot (WCDB 2098:219). A few years later, the warehouse was reported to be vacant (RCD 1975). Various businesses made use of the building in the following decades, including Jullian's Billiard Café (WCDB 8221:86). The large deck and outdoor bar are presumed to have originated from this later occupation in the late 1990s and early 2000s.



Figure 5: Detail of 1959 Photograph Highlighting Warehouse at 411 W. Morgan Street (United States Department of Agriculture [USDA] Historic Aerial Photos, UNC Library 2012).



11(b): Capital Coca-Cola Bottling Company (CCCBC) Garage

117–121 S. West Street  
Raleigh, North Carolina  
PIN: 1703580676



*Physical Description*

Formerly an office and garage building for the CCCBC, the one-story, rectangular, masonry building located at 117–121 S. West Street was constructed in 1949 (Photo 10). The continuous foundation is not visible, but its masonry walls are composed of concrete block clad in a five-to-one, common-bonded brick veneer. Along the south elevation, brick pilasters define the building's eight bays, and likely supported truss members from the adjacent building that historically shared this wall with the building of study (Photo 11, p. 17). Historically, the south elevation abutted another building. The low-pitched, rounded roof is surrounded by a stepped parapet wall topped by a tile coping. No chimneys were visible at the time of survey.



Photo 10: View of West Elevation, 117–121 S. West Street, Looking South Southeast.



Photo 11: Southeast Oblique, 117–121 S. West Street.

Three entries are accessed from the west elevation, or primary façade, two of which are recessed from the plane of the wall. The two recessed entries are positioned at either end of the façade, and appear to be later additions. The third entry is located near the north end of the west elevation and fills what used to be a large open bay for automobiles and trucks. Today, this entrance contains a set of double-leaf metal doors, a single-leaf door of the same material, corrugated metal siding, and two fixed, metal-framed windows (Photo 12). Another historic garage opening, situated near the southern end of the façade, has also been framed in and filled with corrugated metal, a fixed window, and a solid metal panel.



Photo 12: Detail of North Entries to 117 S. West Street, West Elevation.  
Note curb cuts in sidewalk that facilitated automobile access to the building.



The rear or east elevation of the building contains one entrance, located at the southeast corner and accessed by a set of wood stairs and ramp. It is filled by a single-leaf, solid metal door that is flanked by 10-light, steel-framed windows with a central, four-light, awning sash (Photo 13). The panes in these two windows and the other window at the east elevation—a 20-light, steel-framed unit with central awning sash—are all painted white. Several metal vents and electrical conduits mark the northern end of the east elevation (Photo 14). A couple of historic window openings at the rear appear to have been filled with brick over time.

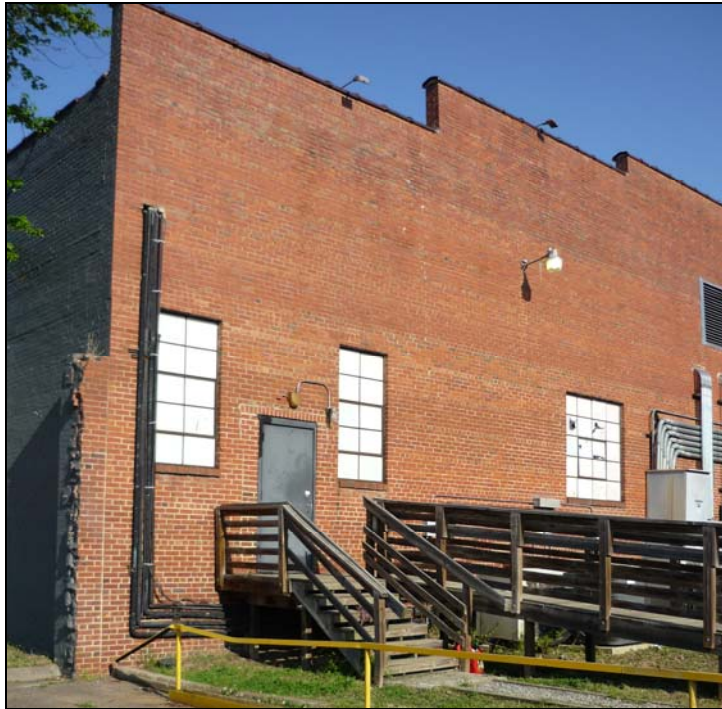


Photo 13: Detail of Rear Entry and Other Fenestration at Southeast Corner. Note remnant of adjacent building wall at left, now demolished.



Photo 14: View of Rear, or East, Elevation Looking West-Southwest.

The interior of the building at 117–121 S. West Street was not accessed at the time of survey.

The setting of this resource remains much the same since it was constructed in the mid-twentieth century, but the demolition of neighboring buildings have left the building more exposed in recent decades (Figure 6). Some paved and gravel parking lots fill the voids left on the south and east sides of the property. Along the façade, the building continues to be lined with wide concrete sidewalks and a couple of street trees.

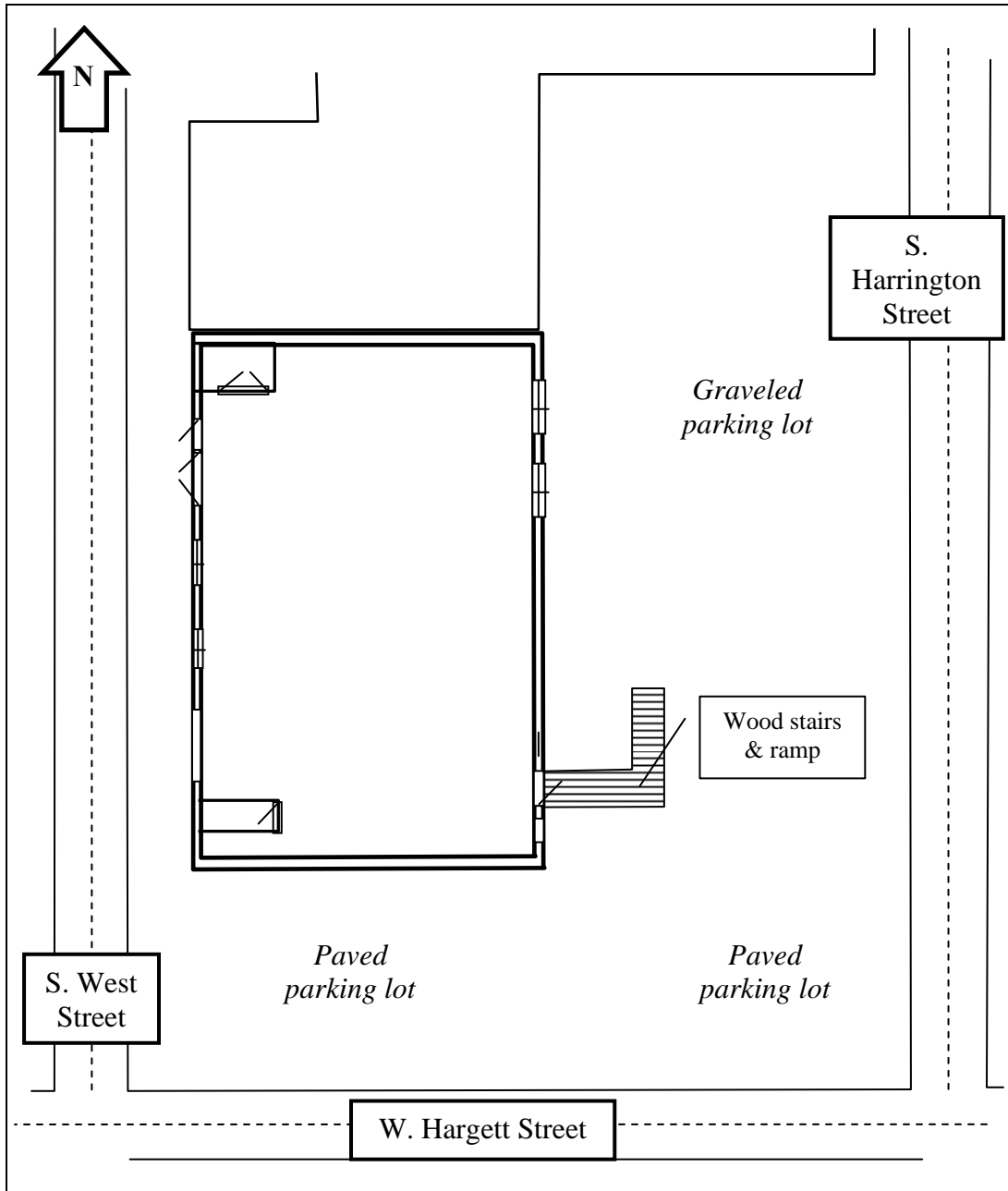


Figure 6: Site Plan, 117–121 S. West Street. Not to scale.

Recent modifications, particularly those along the building's façade and rear elevation, have somewhat diminished the historic character of this resource. This includes the infill of original openings and frequent use of replacement materials, as well as alterations to the historic points of entry, particularly the garage bay openings. No signage or marking on the building illustrate its association with the CCCBC, or its historic function as a garage. The building's historic massing and industrial character remain relatively intact. On the whole, the building retains a high level of integrity in location, setting, and feeling, but a moderate to low level of integrity in design, materials, workmanship, and association.

### *History*

Initially known as the Raleigh Coca-Cola Bottling Company, the first building the company erected in this area of town was located at 216 S. West Street and detailed in a September 1915 newspaper article as a two-story brick building (Elizabeth Reid Murray Collection, Box 82:18, RCD 1915–1916). Previously, the company had been listed at the corner of Halifax Road and Lane Street (RCD 1913–1914). By 1924, they had constructed another building and relocated to 511 W. Morgan Street, and within little more than a decade, expanded operations there to fill 515 W. Morgan Street and 507 (noted as “garage”) (Figure 7). The business also changed names in this period, becoming the CCCBC (RCD 1924–1937).



Figure 7: Historic Photograph of 507–515 W. Morgan Street Labeled “Capital Coca Cola Bottling Company Exterior 1941” (On file, North Carolina State Archives Photographic Collection). Note Morgan Street bridge over railroad tracks at far right.

This period of growth and expansion appears to have resumed following the end of World War II, when the CCCBC began purchasing many of the residential lots in the vicinity of their headquarters on W. Morgan Street. This included L. C. and Louise Murchinson's land, now 117 S. West Street, in December 1947 and the Riggins' the lot at the northeast corner of S. West and W. Hargett Street in September 1948 (WCDB 984:236, 1002:589).

Although tax records date this building to 1956, the 1949 RCD indicates that the first occupants of 117–121 S. West Street, Peden Steel Company and Coca-Cola Bottling Company, used the space as a warehouse and garage (respectively). Just one year later, Sanborn maps indicate that the Coca-Cola Bottling Company occupied the whole building, using the portion at 117 S. West Street as a “private garage,” and the southern half at 121 S. West Street as “Beverage Machine Storage” (Figure 8). The building depicted in the 1950 Sanborn map is in keeping the physical features identified at 117–121 S. West Street today. Therefore, it is believed that this resource dates to 1949.

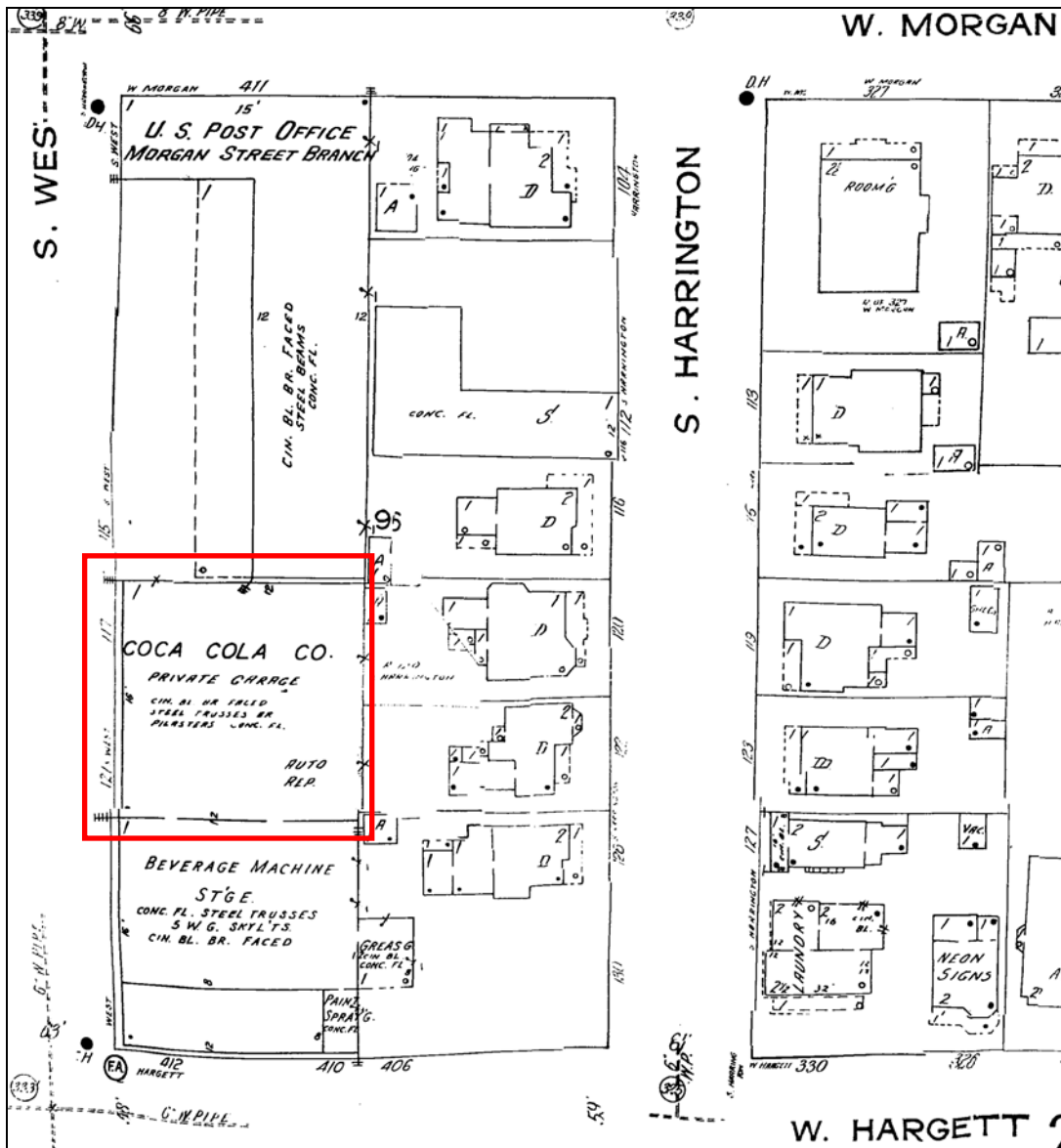


Figure 8: Detail of Sheet 12, 1950 Raleigh Sanborn Fire Insurance Map (Proquest 2001). Property of study highlighted in red.

The CCCBC occupied the building at 117–121 S. West Street throughout much of the 1950s, using it as a garage and office space, but by 1957, they had sold the building to the Gulf Oil Corporation. In December of 1957, Gulf Oil sold the northern half of the CCCBC building to prominent local booksellers and brothers, B. Grimes Williams and Alfred Williams (WCDB 1301:313). Following the death of their father in 1937, the Williams brothers turned their father’s prominent bookselling business, Alfred Williams & Co., into an office supply company and may have used the property for storage and rental income. The building stayed in the Williams’ family until 1988, when it was sold to Erikson Photography (WCDB 4182:263). Archival data and aerial imagery suggest that the southern portion of the building, addressed at 131 S. West Street, was demolished sometime between 1980 and 1993 (Google Earth 2010, RCD 1980–1985).

*NRHP Assessment of 411 W. Morgan and 117–121 S. West Street as Part of District*

The Depot Historic District (WA-0724) was listed on the NRHP in 2002 and determined to be locally significant under Criterion A for its role in industry, transportation, and commerce as the City’s distribution center at the junction of three railroads, and also under Criterion C for its significant concentration of commercial, industrial, and rail-related architecture. The period of significance established for this resource extends from the 1880s until 1952, highlighting “the only significant collection of rail-related buildings, factories, and warehouses in Raleigh that date to the period when railroads predominated in freight transportation” (MAA 2013:24).

According to its NRHP nomination form, the general setting and feeling of the Depot Historic District is characterized by “...blocks of warehouses, factories, and commercial buildings, interspersed by vacant parcels used as parking lots for downtown workers” (Little 2002). At the time of listing, the 29 contributing architectural resources identified within the district largely consisted of “Buildings, ranging from one to three-stories in height, are generally of load-bearing brick construction, with flat roofs, and utilitarian industrial or commercial design, with loading doors, small windows, and modest decorative brick details” (Little 2002).

The nomination further observed that distinctive architectural features of contributing resources include “covered loading docks, utilitarian windows, decorative brick parapet cornices, and modest stylistic elements from the Romanesque Revival to the Art Moderne” (Little 2002). Nine non-contributing resources in the district had been compromised by major modifications made in the second half of the twentieth century.

In early 2013, the HPO approved a boundary expansion for the Depot Historic District as presented in report authored by MAA and entitled *Historic Architectural Resources Survey Report: Raleigh Train Station and Track Configurations, Wake County (P-3803)*. This expansion added 10 historic properties to the district, all of which are located at the northwest corner of the 2002 boundaries on the east side of the Boylan railroad wye. These resources consist of one- and two-story distribution warehouses and commercial buildings constructed between 1914 and the late 1940s.



Possessing a moderate to low level of historic integrity, the two mid-twentieth-century architectural resources selected for further study at 411 W. Morgan and 117–121 S. West Street are recommended not eligible for individual listing on the NRHP. However, both of these resources were constructed within the period of significance prescribed for the Depot Historic District, and each property retains many of the character-defining architectural features contained within it. As seen in a 1959 historic aerial, the USPO and CCBC buildings are in keeping with the historic use and design of this area within the City (Figure 9).

Therefore, the architectural properties at 411 W. Morgan Street and 117–121 S. West Street are **recommended as contributing elements of the Depot Historic District**, and it is further recommended that the district boundaries be expanded to include them (Figure 10, p. 24).

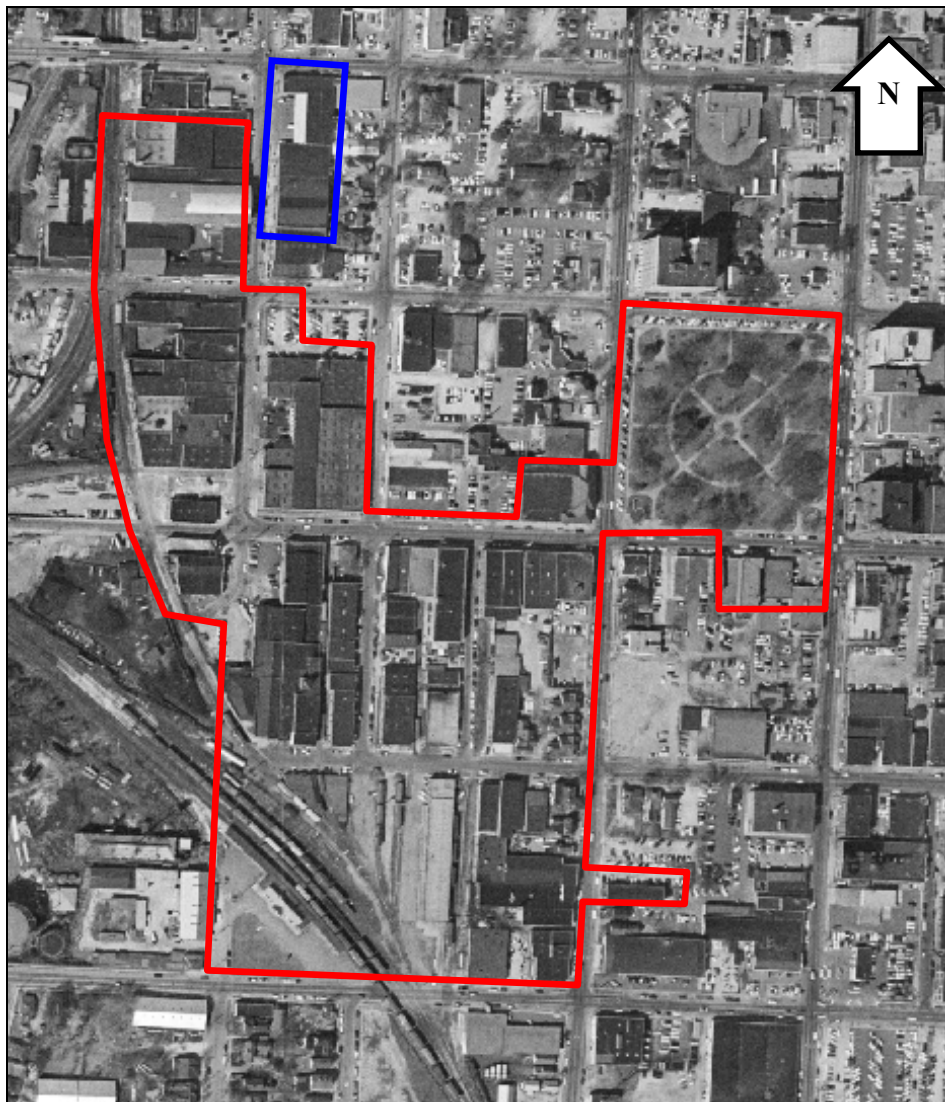


Figure 9: Detail of 1959 Aerial Photograph Highlighting Depot Historic District Boundary Including 2013 Expansion in Red (USDA Historic Aerial Photos, UNC Library 2012). Resources of study highlighted in blue.



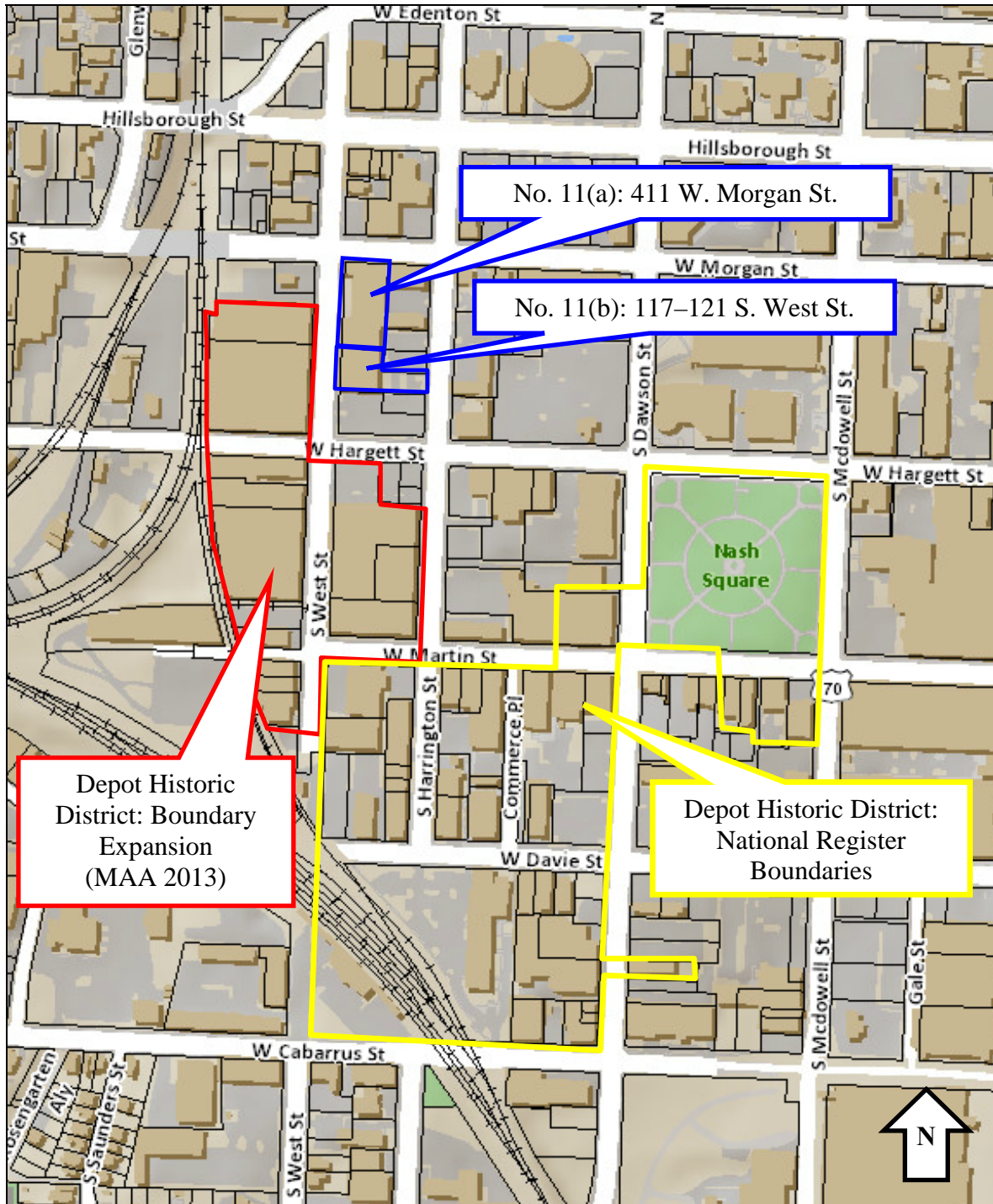


Figure 10: Depot Historic District NRHP Boundaries, Including 2013 Expansion (Red) and Additional Resources (Blue) (City of Raleigh and Wake County iMaps [iMaps] 2012; MAA 2013).

## Resources Recommended Not Eligible for the NRHP

### *No. 8: Woodard's Gulf Service Station*

520 W. South Street  
Raleigh, North Carolina  
PIN: 1703465311/1703464385



#### *Physical Description*

M. W. Woodard's one-story, four-bay Gulf service station is currently addressed at 520 W. South Street and occupied by the Men at Work Detailing Company (Photo 15). Situated at the northeast corner of the intersection between W. South and S. Saunders Streets, the resource currently encompasses two lots: 520 W. South Street and 611 S. Saunders Street. The original portion of the building rests on a poured-concrete slab foundation with walls constructed of two brick wythes laid in a six-to-one common bond. Along the south and west elevations, the masonry structural system is covered by metal panels that have been routinely painted. The station building is covered by a flat or low-pitched shed roof to which wide overhanging eaves and a rectangular metal canopy was later added. A small brick flue is located near the northwest corner of a circa-1950 addition at the rear.



Photo 15: Southwest Oblique, 520 W. South Street.

The main entrance is situated under the canopy on the primary façade, or south elevation. Today, this entry is filled by a contemporary, single-leaf, metal door with a fixed rectangular light filling much of its upper half. Above this entrance, an area historically filled with a transom window is now marked by a wood panel. Two additional single-leaf entries, located at the northwest corner of the building, provided external access to the restrooms, both of which are now in-filled or boarded over. Three garage bays fill the east end of the façade, each of which contain a six-light, nine-metal-paneled, steel-framed overhead door.

Other patterns of fenestration in the original portion of the building include two single-pane, fixed, display windows of recent vintage at the southwest corner of the station—each of which are topped by two wood panels that are also recent replacements. Two four-light, metal-framed casement windows dot the north end of the west elevation, but have been covered with white paint (Photo 16). Centrally located on the east elevation, four windows with brick sills are currently boarded over.

A one-story, cinder-block addition extends across much of the rear, or north, elevation of the station and appears to have been added in the 1950s (Photo 17). A single garage bay fills the west elevation of this addition, while a small doorway and window opening, located on its east elevation, have been in-filled or boarded over.



Photo 16: West, or S. Saunders Street, Elevation.



Photo 17: Northeast Oblique, 520 W. South Street.



At the interior of the building, the spatial organization of the original sales area has been somewhat modified. Filling the western bay of the building, the sales area has been expanded, and a few interior walls removed, leaving a large, open, office space and one bathroom at the back (Photo 18 and Photo 19). Original exterior doors to the bathroom have in-filled or boarded over. The original three-bay service area continues to be free from obstruction (Photo 20, p. 28). Some of the original drains in the service area floor remain.



Photo 18: View of Sales Space, Looking South.

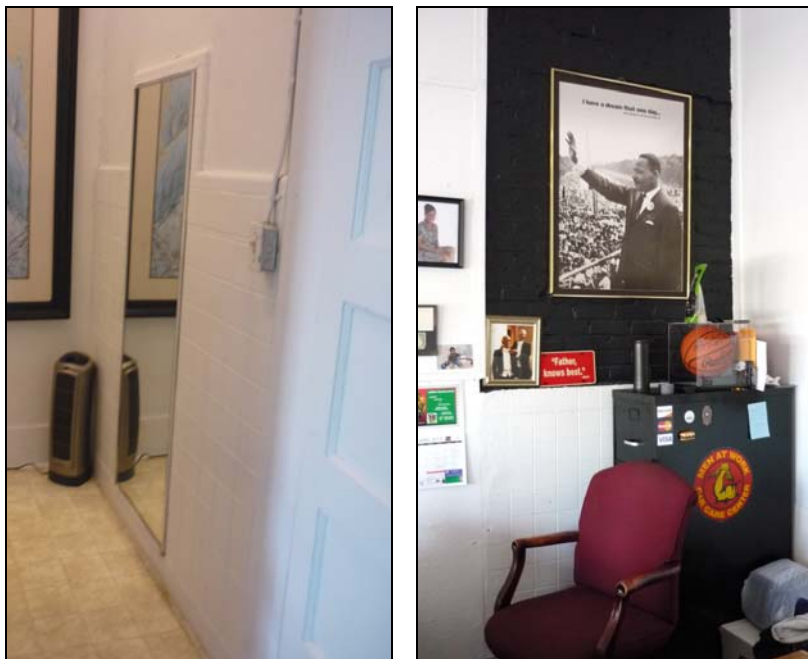


Photo 19: At Left, View of Extant Bathroom with Original Tile and Mirror; At Right, View of Former Bathroom Area Showing Portions of Original Tile at Lower Half of West Wall.



Photo 20: View of Service Area, Looking Northeast.

At some point after the rear addition was made to the building, a new opening was made in the north wall to facilitate passage between the addition and original building. Although undoubtedly auto-related, it is unclear how the addition originally functioned (Photo 21). A portion of its north wall appears to rest on an older continuous stone foundation, but many of its architectural features have been modified over time, including some alterations around the single garage bay opening at the west elevation (Photo 22, p. 29).



Photo 21: Interior of Rear Addition, View Looking West.



Photo 22: View of Northwest Corner, Showing Garage Entry at Rear Addition.

Today, much of the station building is surrounded by concrete and asphalt pavement, with a stepped, brick and concrete-block retaining wall running along the eastern boundary of the parcel (Figure 11, p. 30). The rear addition is partially banked into the ground along the north elevation, where the change in topography is further defined by brick and concrete-block planters that also function as retaining walls to some extent. Other poured-concrete features remain visible, such as the rounded sidewalk that borders the building itself and the former pump island, although the later has been somewhat modified (Photo 23).



Photo 23: View of Former Pump Island and Canopy Posts, Looking East.

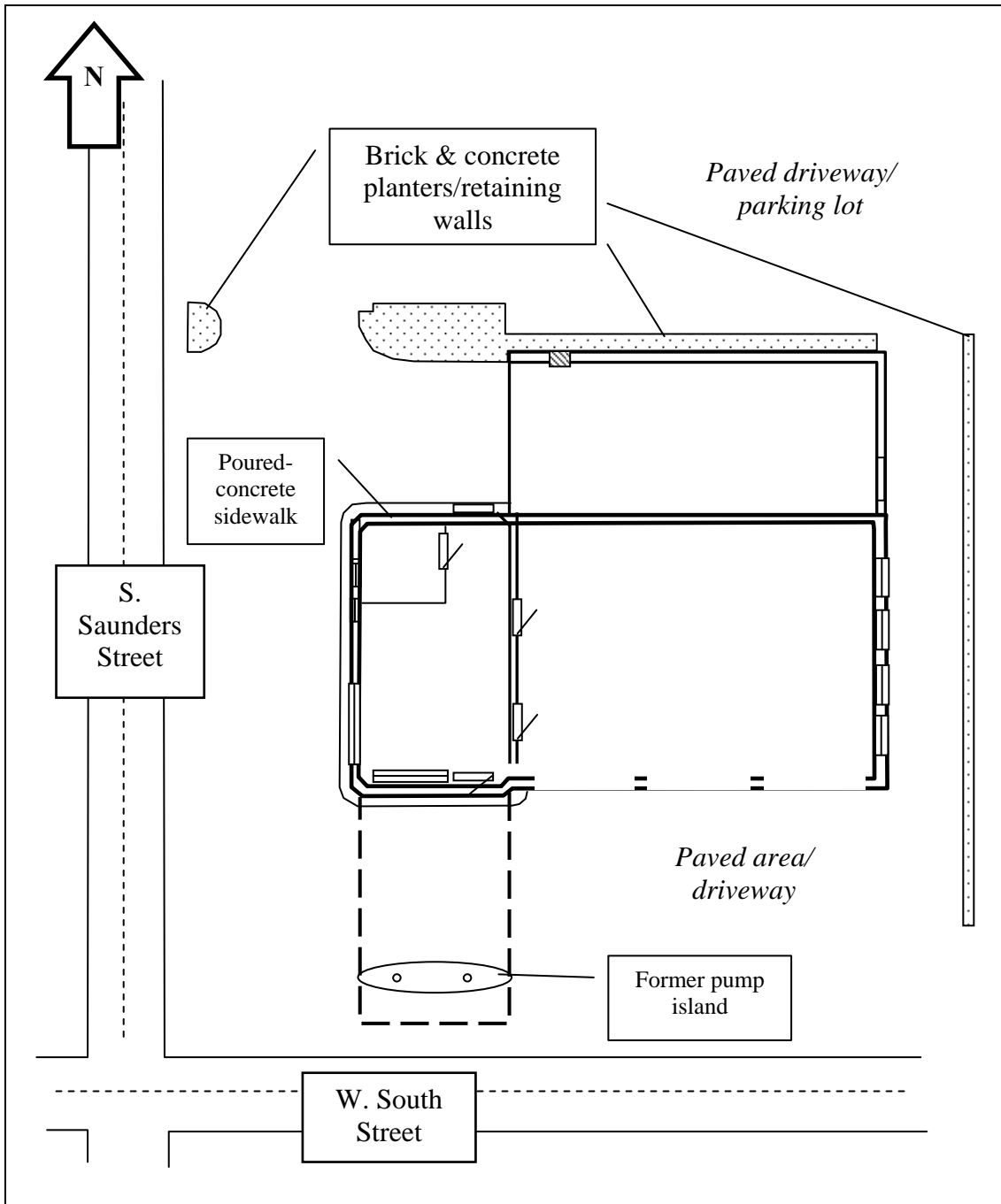


Figure 11: Site Plan of 520 W. South Street. Not to scale.

The historic integrity of Woodard's Gulf Service Station has been diminished by alterations to the building and surrounding lot over time. Modifications include the rear concrete-block addition, removal of an original bathroom and alteration of sales area, replacement of display windows and transoms, garage doors and pedestrian entry, as well as the loss of gas pumps.



### *History*

The service station building at 520 W. South Street was constructed in 1939 or 1940 and operated by Macon W. Woodard for more than 40 years. Woodard, and his wife Mary Frances, purchased two residential lots in what had started out as a late-nineteenth century African American neighborhood in July 1939 (WCDB 810:440, 448). Dwellings appear on each lot in a 1938 aerial image of the area, but were demolished soon after this sale. Woodard's station first appeared in the 1941 RCD addressed at 522 W. South Street. In 1942, Gulf Oil Products distributor, W. G. Weems, placed an ad in the local directory identifying 13 Gulf dealers in town, including M. W. Woodard at "South and Saunders Streets" (RCD 1942).

In the decade prior to purchasing the lots for his own gas and service station, Macon Woodard was employed at other filling stations in town, including the Ideal Service Station and Pope's Service Station and Tire Company (RCD 1926–1940). He and Mary did not appear in Raleigh's city directories before 1926. Census records indicate that Macon Woodard lived in south-central Wake County as a child and into his young adulthood. At the age of 25 in 1930, Macon owned the house he shared with his 24-year-old wife, Mary, and his mother, Roxie, in House Creek township, in a rural area south of the city. By 1940, the couple had moved to Cary, residing on Western Boulevard with their young daughter, Joyce Ann (U.S. Census).

A 1949 photograph of Woodard's Service Station, found in the State Archives' Photographic Collection, reveals an earlier International style Gulf Company design (Figure 12; Figure 13, p. 32). The original form, fenestration patterns, poured-concrete features, and some exterior metal panels from this initial corporate design remain visible in the station building today.



Figure 12: Historic Photograph Showing Primary, or South, Elevation of Woodard's Service Station, Circa 1949 (On file, North Carolina State Archives Photographic Collection).



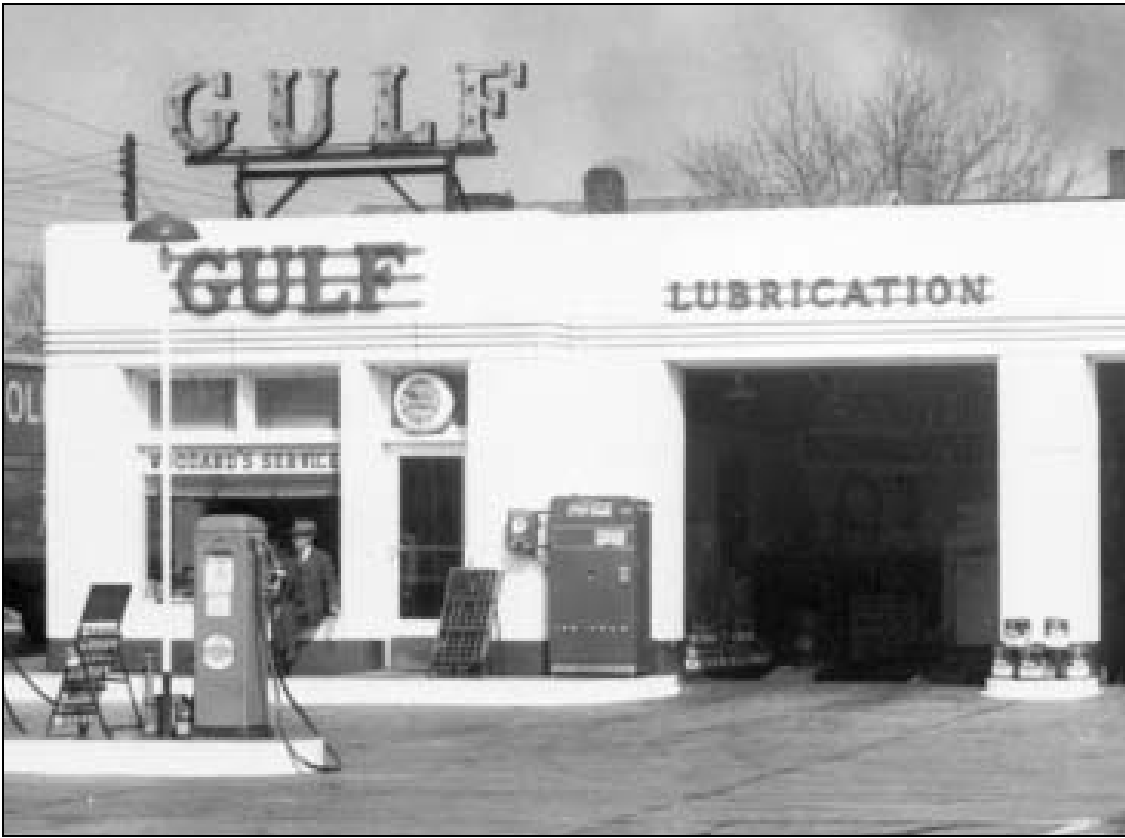


Figure 13: Detail of 1949 Photograph Showing Main Entry and Pump Island (On file, North Carolina State Archives Photographic Collection).

Raleigh's city directories identify M. W. and Mary Woodard with the service station on W. South Street and dwelling on Western Boulevard from 1941 into the early 1980s. Historic aerial images suggest that the rear addition was made sometime in the 1950s (Figure 14, p. 33). Years later, the station building appears to have undergone at least one corporate-inspired renovation. A November 1965 amended lease agreement indicated that Woodard had agreed to make improvements to the property at his own expense "at an estimated cost of \$7,886.00" in exchange for another 10 years on his lease with the Gulf Oil Corporation (WCDB 1717:199). It is believed that these improvements included the addition of the canopy and wide overhanging eaves to bring the station's exterior image up to date with corporate expectations. Woodard operated the station until the early 1980s when it was listed as Duke's Union 76 station in city directories. In August 1989, Woodard, then a widower, sold the station and lot to Barney G. Joyner, whose family trust continues to manage the property (WCDB 4554:121).

In the early 1930s, a Standard Oil gas station was identified at the southeast corner of W. South and S. Saunders Street (RCD 1932). By the time Woodard's Service Station was in operation, this cross-street competitor had become an Esso station (RCD 1941). Though the Esso station building is no longer extant, these two businesses coexisted at this intersection into the late 1960s.



Figure 14: Detail of 1959 Aerial Showing Rear Addition to Station Building (USDA Historic Aerial Photos, UNC Library 2012). Property of study highlighted in blue.

#### *NRHP Assessment*

At the request of the HPO, a handful of historic service stations in the city were physically inspected and surveyed to place this resource in an appropriate context. These resources included the stations at 623 and 630 W. Hillsborough Street, 3120 Hillsborough Street, 600 W. Peace Street, and 2001 Wake Forest Road. This reconnaissance survey revealed that only one of these stations is still in use for both automobile service and gasoline: 630 W. Hillsborough Street. A conversation with the station owner and operator revealed that this building was constructed in 1970 as a Sunoco station. Three other stations continue to provide some kind of service related to automobiles and one is vacant (623 W. Hillsborough Street). Only the functional gas station at 630 W. Hillsborough Street retains gas pumps.

In addition to these properties, Dovetail identified another station from this period at 3020 Hillsborough Street, as well as a previously examined circa-1958 Gulf station at 915 Capital Boulevard and former Esso station at 2121 Garner Road (Barile et al. 2012). The current investigation and scope of work does not include the creation of a complete historic context for gas stations in the City; however, a review of these properties provides a better concept of this particular property type in Raleigh (Photo 24–Photo 26, p. 34; Photo 27–Photo 29, p. 35; Photo 30 and Photo 31, p. 36).



Photo 24: Circa-1935 Gibbon's Esso Station, 623 W. Hillsborough Street (WA-2887).  
Determined Eligible for NRHP in 1999 and Recently Renovated.

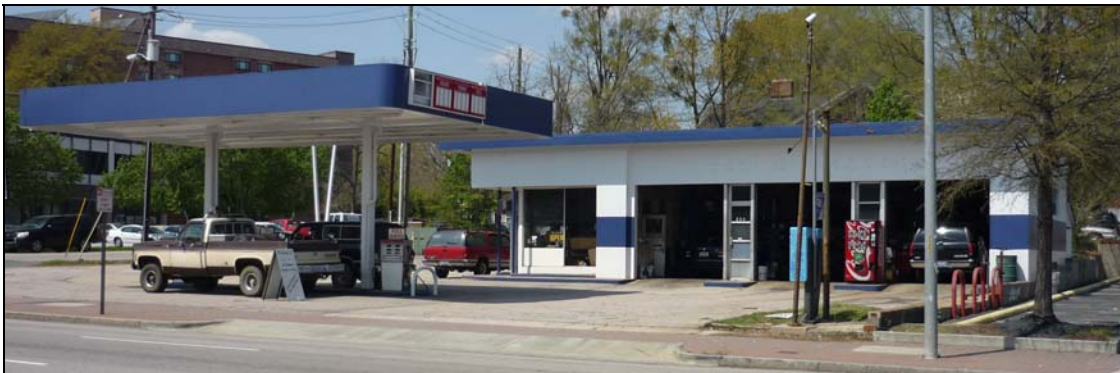


Photo 25: Circa-1962 Sunoco Station at 630 W. Hillsborough Street (RCD 1962).



Photo 26: Circa-1940 Wilmont's Service Station 3120 Hillsborough Street (WA-2902).





Photo 27: Circa-1955 Esso Station at 600 W. Peace Street (RCD 1962).



Photo 28: Circa-1940 Texaco Station at 2001 Wake Forest Road (RCD 1954).



Photo 29: Circa-1959 Gulf Station at 3020 Hillsborough Street (RCD 1960).



Photo 30: Circa-1958 Gulf Station at 915 Capital Boulevard (WA-7363),  
Determined Not Eligible for NRHP in 2012.



Photo 31: Circa-1950 Former Esso Station at 2121 Garner Road, Recently Renovated.

Gas stations were noted as an “important suburban building type” in Little’s 2006 Modernist context for the City of Raleigh, although just one example was surveyed in association with the study. This resource, an Esso station at 2121 Garner Road and pictured above, was prized for its “flat-roofed box with white enameled steel sheathing...” among other Modern architectural features, but has recently renovated using a substantial amount of replacement materials (Little 2006:33).

Additional research on mid-century gas station design uncovered W. Dwayne Jones’ 2003 publication for the Texas Department of Transportation (TXDOT) entitled, “A Field Guide to

Gas Stations.” This contextual study reported on the gas station’s historical development during the twentieth century, detailed the architecture of several important oil companies, and established registration requirements for these resources within the state of Texas.

Another study of these property types, presented by Monica Penick and Gregory Smith in their multiple property submission nominating Route 66 for the NRHP, draws heavily from John Jakle and Keith Sculle’s study, *The Gas Station in America*. Jakle and Sculle note the use of an “oblong box” in service station design that persisted from 1930 until 1960 (Penick and Smith F:15). They differed from their predecessors as:

These stations no longer featured hip or gable roofs or historically-inspired architectural details, but—perhaps in line with the growing interest in Modern architecture and the International Style—the gas station became a flat-roofed rectangular box with little ornament and large expanses of plate glass (Penick and Smith F:16).

It was further observed that, “The larger gas stations with three or more bays were often called ‘super service stations,’ and advertised ‘one stop’ automotive service for washing, lubricating, engine brake and muffler repair (Penick and Smith F:16). These super stations were often fitted within the oblong box form, and thrived during the Depression era when gas sales dwindled and the need for repairs and replacement parts grew. Initially, Woodard’s Service Station appears to have been one of the few such “super” stations in town. Historically known as Wilmont’s Service Station, the building at 3120 Hillsborough Street also appears to have possessed three service bays, two restrooms, and a large sales area.

Today, the architectural features exhibited at 520 W. South Street reflect a standard building type of the Gulf Oil Corporation that was popular during a period of great expansion for the company from 1950 until the early 1970s (TXDOT and Jones 2003:87) (Figure 15, p. 38). Significant components of this station type built upon an earlier corporate design, dating to the late 1940s, that exhibited International or Modern styling, an oblong box form, a flat roof “with three distinctive blue bands encircling the building,” and white-enameled porcelain sheathing” (TXDOT and Jones 2003:68). By the 1960s, Gulf Company stations frequently featured two service bays, a flat-roofed canopy resting on two slender metal poles, large corner windows at the sales space, and a “second detached island of gasoline pumps...” (TXDOT and Jones 2003:87) (Figure 16, p. 38).

The three bands encircling the building were seen in the historic image of Woodard’s Service Station, but are no longer extant. White-enameled paneling remains along the south and west elevations, but have been frequently painted over the years. The circa-1965 canopy and angular eaves illustrate later modifications that adhere to the company’s changing aesthetic, but few other architectural features remain to illustrate the station’s corporate design. Though its integrity of location, setting, feeling, and association remain intact, modifications to the building’s original plan and moderate use of replacement materials have negatively affected its integrity of design, materials, and workmanship.



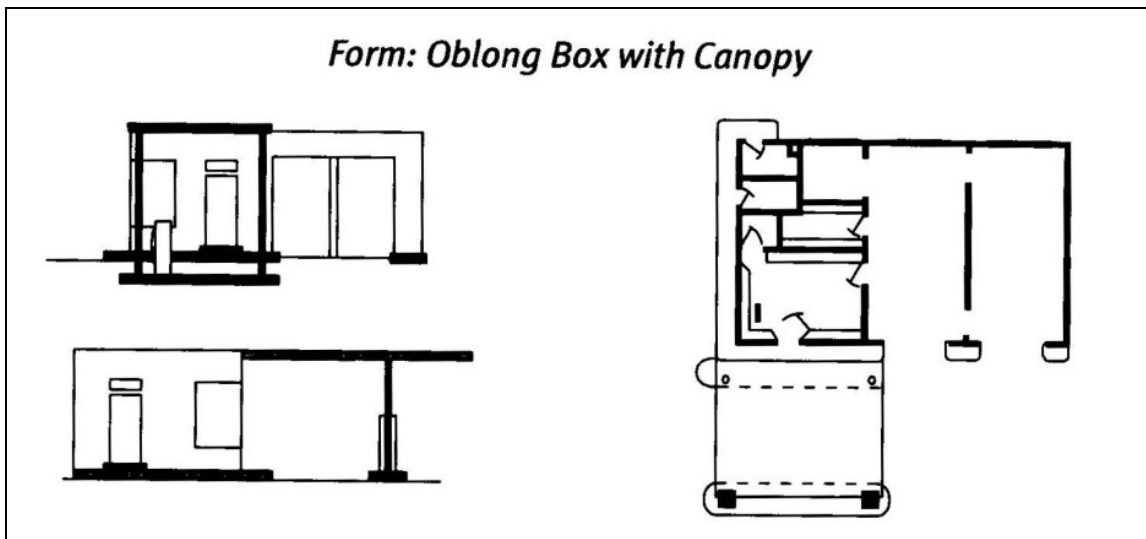


Figure 15: Architectural Drawing of Oblong Box Form Commonly Employed in Gulf Company Standard Station Design, 1950–1970 (TXDOT and Jones 2003:87).

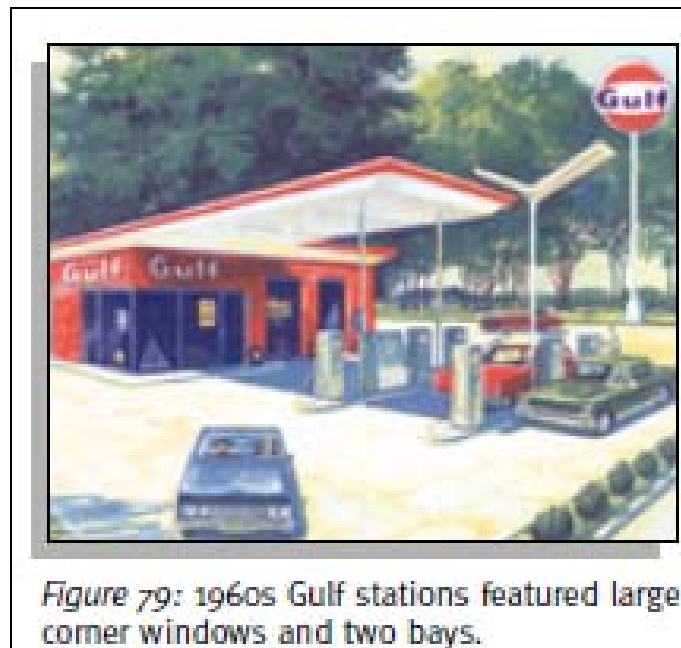


Figure 79: 1960s Gulf stations featured large corner windows and two bays.

Figure 16: Image and Caption, Description of Gulf Station Design (TXDOT and Jones 2003:87).

In a previous investigation led by Dovetail, the circa-1959 Gulf gas station at 915 Capitol Boulevard was determined to be not eligible for the NRHP due to “the loss of the original bay doors, entrance doors, lighting fixtures, and gas pumps [that] have compromised the integrity of design, materials, and workmanship” (HPO Memo of May 9, 2012). Woodard’s Gulf Service Station has undergone similar modifications, including a rear addition, removal



of one of two original bathrooms and spatial alteration of sales area, replacement display windows and loss of transom windows, replacement of garage and main entry doors, as well as the loss of gas pumps and light fixtures. As such, the resource at 520 W. South Street is recommended not eligible for individual listing on the NRHP under Criterion C.

Macon W. and Mary F. Woodard are not known to have made significant impacts upon national, state, or local history, nor does this property have any significant associations with important events in the past. Therefore, this resource is recommended not eligible for the NRHP under Criteria A or B.

For a property to be eligible under Criterion D, it must meet two requirements: 1) the property must have, or have had, information to contribute to our understanding of human history or prehistory, and 2) the information must be considered significant. Due to the large-scale modifications of the surrounding landscape during land development at the time of the business' establishment, subsurface deposits throughout this area have been greatly disturbed through the installation of subsurface gas tanks and other utilities. Moreover, the commonality of mid-century construction precludes the potential for below-ground remains associated with this resource to provide notable data on commercial structural technology in this area. As such, it is recommended that the resource is not eligible under Criterion D.

In sum, Woodard's Gulf Service Station at 520 W. South Street is **recommended not eligible under Criteria A–D.**

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**APPENDIX A: BUILDING INVENTORY**

**West Street Project Building Inventory, December 2012**

| <b>SURVEY #</b> | <b>HPO #</b> | <b>PROPERTY NAME</b>                        | <b>ADDRESS</b>  | <b>NRHP ELIGIBILITY</b>                       |
|-----------------|--------------|---|---|---|
| 1               | WA-0195      | Boylan Heights Historic District            | Dorthea Drive, Western Boulevard, Florence Street, and Dupont Circle, Raleigh | Listed on the NRHP under Criteria A, B, and C |
| 2               | WA-0724      | Depot Historic District                     | W. Martin Street, Nash Square, S. Dawson Street, S. West Street, Raleigh      | Eligible for the NRHP under Criteria A and C  |
| 3               | WA-4082      | Old Fourth Ward Historic District Expansion | W. Cabarrus Street, S. West Street, Raleigh                                   | Not Eligible                                  |
| 4               | WA-4185      | S. Boylan Avenue Historic District          | Hillsborough Street, W. Hargett Street  | Not Eligible                                  |
| 5               |              | Commercial Building                         | 320 W. South Street, Raleigh  | Not Eligible                                  |
| 6               | WA-3124      | Commercial Building                         | 416 W. South Street, Raleigh  | Not Eligible                                  |
| 7               |              | Commercial Building                         | 518 W. South Street, Raleigh  | Not Eligible                                  |
| 8               |              | Gas Station                                 | 520 W. South Street, Raleigh  | Recommended for further study                 |
| 9               |              | Office Building                             | 600 W. Cabarrus Street, Raleigh   | Not Eligible                                  |
| 10              |              | Warehouse                                   | 414 Dupont Circle, Raleigh  | Not Eligible                                  |
| 11              |              | Warehouse                                   | 411 W. Morgan Street/117 S. West Street, Raleigh                              | Recommended for further study                 |
| 12              |              | Commercial Building                         | 100 S. Harrington Street, Raleigh   | Not Eligible                                  |

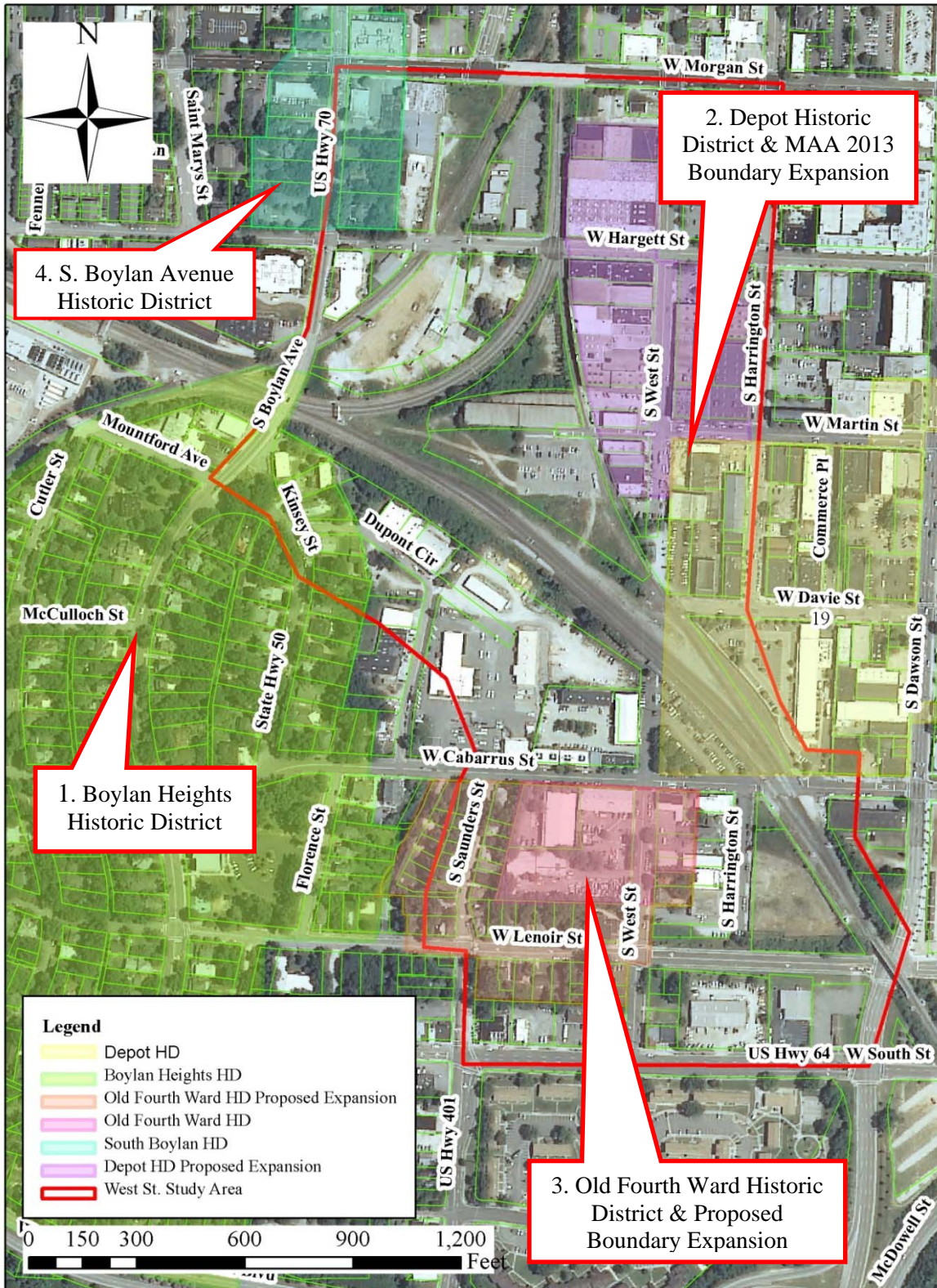


Figure 17: Resources 1–4 Surveyed as Part of a Historic District.





Figure 18: Map of the West Street Extension Project Study Area Showing Resources 5–12.



## Copy of HPO Correspondence, March 18, 2013



North Carolina Department of Cultural Resources  
State Historic Preservation Office  
Ramona M. Bartos, Administrator

Pat McCrory, Governor  
Susan W. Kirtz, Secretary  
Kevin Cherry, Deputy Secretary

Office of Archives and History  
Division of Historical Resources  
David Brook, Director

March 18, 2013

Eric Lamb  
Office of Transportation Planning  
PO Box 590  
Raleigh, NC 27601  
eric.lamb@raleighnc.gov

Re: Draft Historic Architectural Resources Survey, West Street Extension from Cabarrus Street to Martin Street, Raleigh, U-5521, Wake County, CH 12-1829

Dear Mr. Lamb:

On February 28, 2013, Suzanne Unger Young from Michael Baker Engineering, Inc. transmitted the draft survey report prepared by Dovetail Cultural Resources for the City of Raleigh for the above project.

For the purposes of compliance with Section 106 of the National Historic Preservation Act, we concur that the following properties are currently listed in, and remain eligible for listing in, the National Register of Historic Places under the criteria cited, and that the existing National Register boundaries remain appropriate:

- **Boylan Heights Historic District (WA 0195)**, Criterion A for community planning and education, Criterion B for its association with Frank Ellington and J. Stanhope Wynne, and Criterion C for architecture.

We concur that the existing boundaries of the **Depot Historic District (WA 0724)**, currently listed in the National Register, and the 2013 expansion of the historic district (see Mattson, Alexander, and Associates' Final Historic Architectural Resources Survey Report for the Raleigh Rail Union Station-Phase I and Track Improvements, P-5500, ER 12-0560) are eligible for listing in the National Register under Criterion A for industry, transportation, and commerce, and Criterion C for architecture. However, we believe that the buildings at 411 West Morgan Street and 117 South West Street (identified collectively in the report as Property #11) warrant additional research to determine if they are eligible for inclusion in a possible expansion of the Depot Historic District. The 1950 Sanborn maps show these buildings as the US Post Office, Morgan Street Branch and as a garage for the Coca-Cola Company respectively.

At this time, we believe that the **Old Fourth Ward Historic District (WA 4082)** is not eligible for listing in the National Register and additional research on this district is not recommended. Although the district was placed on the State Study List in 1989, the non-residential properties near the intersection of Cabarrus and West Streets were found ineligible in the 2013 Mattson Report. In 2010, a separate Study List Application for Rosengarten Park (generally along Saunders Street, between Lenoir and Cabarrus Streets) was submitted to our office. After review by staff, it was determined that this area lacked sufficient integrity, primarily as a result of numerous demolitions over the years. The integrity of the remaining portions of the Old Fourth Ward Historic District has also been severely compromised since it was placed on the Study List.



We recommend that additional research be conducted regarding the Gas Station (Property #8) at 520 West South Street. In May 2012, our office reviewed an architectural survey report prepared by Dovetail for NCDOT for the replacement of bridges along Capital Boulevard. At that time, we identified several oblong box form gas stations, including: Gibbon's Esso Service Station (WA 2887, built circa 1935) at 623 West Hillsborough Street, Wilmont Service Station (WA 2902, built circa 1940) at 3120 Hillsborough Street, the former service station at 501 West Morgan (WA 3008, built circa 1930), the service station at 630 Hillsborough Street (built circa 1952), the former service station at 600 West Peace Street (unknown date), and the service station at 2001 Wake Forest Road (built circa 1940). Based on the submitted photograph and the Google StreetView images—which do not show the existing bay doors—this gas station appears to retain as much or more integrity than the other previously identified stations.

We concur that, barring additional information to the contrary, the following properties are *not* eligible for listing in the National Register and that no additional study is recommended:

- South Boylan Avenue Historic District (WA 4185);
- Commercial Building, 320 West South Street;
- Commercial Building, 416 West South Street;
- Commercial Building, 518 West South Street;
- Office Building, 600 West Cabarrus;
- Warehouse, 414 Dupont Circle; and,
- Commercial Building, 100 South Harrington Street.

The above comments are made pursuant to Section 106 of the National Historic Preservation Act and the Advisory Council on Historic Preservation's Regulations for Compliance with Section 106 codified at 36 CFR Part 800.

Thank you for your cooperation and consideration. If you have questions concerning the above comment, please contact Renee Gledhill-Earley, environmental review coordinator, at 919-807-6579. In all future communication concerning this project, please cite the above-referenced tracking number.

Sincerely,



*for* Ramona M. Bartos

cc: Martha Lauer, Raleigh Historic Development Commission, [martha.lauer@rhdc.org](mailto:martha.lauer@rhdc.org)  
Mary Pope Furr, NCDOT  
Dovetail Cultural Resources

## Phase I Building Inventory and Evaluations

1. WA-0195: Boylan Heights Historic District  
Bounded by Dorthea Drive, Western Boulevard, Florence Street,  
and Dupont Circle, Raleigh  
PIN: Multiple



The Boylan Heights Historic District is located southwest of the Norfolk and Southern Railroad in Raleigh. The neighborhood was initially developed in 1907 and designed with curvilinear streets and irregularly shaped lots. The dwellings constructed on the parcels are representative of popular architectural styles at the time, including Queen Anne, Colonial Revival, and Craftsman. Previous studies note this district is representative of a “major documented attempt by the city’s controlling interest to accommodate the rapidly growing white middle class of the early 20th century Raleigh” (HPO 1985).

The Boylan Heights Historic District was listed on the NRHP in 1985 under Criteria A, B, and C. Although some alterations have affected individual houses within the Boylan Heights neighborhood, the district as a whole has undergone minor modifications in recent decades. The HPO concurred with Dovetail’s recommendation that this resource remains eligible for and listed on the NRHP.

2. WA-0724: Depot Historic District  
Bounded by W. Martin Street, Nash Square, S. Dawson Street, S. West Street  
PIN: Multiple



The Depot Historic District is located northeast of the Norfolk and Southern Railroad tracks. The district is composed of warehouses and industrial buildings that date from the 1880s to the mid-twentieth century. Nash Square, one of the original landscape features in the 1792 city plan, is also part of this district. The neighborhood exhibits a strong association with transportation and industry in Raleigh.

The Depot Historic District was listed on the NRHP under Criteria A and C in 2002. Although some alterations have impacted individual buildings and structures, the district as a whole has undergone few modifications since it was originally developed. In a 2012 report, MAA proposed an expansion of the Depot Historic District boundaries to include resources on both sides of S. West Street and W. Hargett Street (MAA 2013). The HPO concurred with Dovetail's recommendation that this resource be expanded as proposed by MAA in 2013 and that it remains eligible for and listed on the NRHP.

3. WA-4082: Old Fourth Ward Historic District Expansion  
Bounded by W. Cabarrus Street, S. West Street  
PIN: Multiple



The Old Fourth Ward Historic District is located in southeast Raleigh. This part of the city was developed in the late-nineteenth and early-twentieth century with predominately frame houses initially inhabited by African American residents. Although this district was affected by the urban renewal movement in the third quarter of the twentieth century, the buildings that remain are representative of a post-Civil War African American community and are in good to fair condition. In 1989, this district was placed on the Study List and was composed of seven blocks. In a 2012 report, MAA proposed a boundary reduction to this resource; the district was limited to the northeast corner at the intersection of W. Cabarrus Street and S. West Street (MAA 2013).

In 2012, MAA recommended the Old Fourth Ward Historic District as not eligible for the NRHP under Criteria A and C based on the proposed boundary reduction (MAA 2013). Although Dovetail concurs with MAA's historic significance and their integrity assessment of the existing boundaries, we propose an expansion to the district boundaries that is comparable to the 1989 Study List boundaries. These new boundaries would include the cluster of early-twentieth century dwellings along Rosengarten Alley, S. Saunders Street, and W. Lenoir Street. This area highlights the highest density of surviving historic buildings associated with the Old Fourth Ward neighborhood. Dovetail recommended additional study of this resource. The HPO did not concur with this recommendation and this resource was determined to be not eligible for the NRHP.

4. WA-4185: S. Boylan Avenue Historic District  
Bounded by Hillsborough Street, W. Hargett Street  
PIN: Multiple



The S. Boylan Avenue Historic District is a two-block area along S. Boylan Avenue, W. Hargett Street, and Hillsboro Street located west of the Norfolk and Southern Railroad tracks. The primarily residential neighborhood was initially developed in the early-twentieth century. Throughout the 1900s, a majority of these Queen Anne and Colonial Revival dwellings were demolished and replaced by office and commercial buildings. Many of the residential buildings that remain were converted into multi-family dwellings in third and fourth quarters of the twentieth century.

In a 1998 survey the district was determined eligible for the NRHP under Criteria A and C. In a 2013 Phase II report entitled, *P-3819: Additional Eligibility for Southeast High Speed Rail Corridor, Wake, Vance, and Warren Counties*, Dovetail conducted an intensive-level survey for this resource and recommended the South Boylan Avenue Historic District to be no longer eligible for listing on the NRHP. No HPO form or file was found at the HPO to detail this district, but previous studies based its historic significance on the concentration of early-twentieth century, single-family dwellings. In this investigation, Dovetail concluded that recent demolitions and the influx of commercial buildings over the last half century, has diminished the historic residential character of the district and resulted in a loss of its historic integrity of setting, design, materials, workmanship, feeling, and association. (Barile et al. 2013). As part of the current project, Dovetail concurs with the recommendation that this district is no longer eligible for listing on the NRHP. The HPO agreed with Dovetail's recommendation and this resource is no longer eligible for the NRHP.

5. Commercial Building  
320 W. South Street, Raleigh  
PIN: 1703564375





Constructed around 1956, the primary resource at 320 S. West Street is a one-story, two-bay building. The brick structural system is covered in stucco and the building is capped by a low-pitched, front-gable roof sheathed in asphalt shingles. The primary entrance—a two-paneled, wood door—is off-centered on the south elevation. Other fenestrations include a three-light ribbon window on the primary elevation. Two one-story, rectangular additions extend off the rear (north) elevation of this building. Like the original core of the building, this brick addition is covered in stucco and covered by a metal, gable roof. Metal vents pierce the roof line. A secondary entrance is off-centered on the south elevation of this addition. It is a metal door with an exterior storm door.

This resource is in fair to poor condition and has undergone several modifications during the last-half of the twentieth century. It does not exhibit high artistic value as the work of a master, nor is it an outstanding example of a particular architectural style. Modifications over time have somewhat diminished its historic integrity. It was recommended not eligible for individual listing on the NRHP under Criteria A–C and has not been evaluated under Criterion D. The HPO concurred with this recommendation and found this resource to be not eligible for the NRHP.

6. WA-3124: Commercial Building  
416 W. South Street, Raleigh  
PIN: 1703469278



The primary resource at 416 W. South Street is composed of two two-story commercial buildings built around 1940 and currently connected by a narrow hyphen. The westernmost building has a cinderblock structural system, which is partially clad in vinyl siding and stucco. It is covered by a side-gable roof that is sheathed in asphalt shingles, with a rear ell that extends northward. This part of the building features contemporary six-panel doors and vinyl replacement windows. The easternmost building is also clad in stucco and covered by a flat or low-pitched shed roof lined with a stepped parapet. It is accessed by a door on the south elevation and features contemporary plate-glass windows.

This resource is in fair condition and has undergone several modifications during the last-half of the twentieth century. It does not exhibit high artistic value as the work of a master, nor is it an outstanding example of a particular architectural style. Modifications over time have somewhat diminished its historic integrity. It was recommended not eligible for individual listing on the

NRHP under Criteria A–C and has not been evaluated under Criterion D. The HPO concurred with this recommendation and found this resource to be not eligible for the NRHP.

7. Commercial Building  
518 W. South Street, Raleigh  
PIN: 1703466312



Built around 1957, the primary resource at 518 W. South Street is a one-story commercial building. This masonry building is clad in a variety of materials including horizontal vinyl siding and a brick, stretcher-bond façade. The building is covered by a flat roof. The primary entrance—a set of plate glass doors set within a metal frame—is located on the east elevation. Other fenestrations include a secondary entrance on the north elevation and fixed, single-light windows. A metal canopy extends off the east elevation.

This resource has been heavily altered in recent decades; the original layout and form is no longer visible. The building is in fair condition and does not exhibit high artistic value as the work of a master, nor is it an outstanding example of a particular architectural style. Modifications over time have somewhat diminished its historic integrity. It was recommended not eligible for individual listing on the NRHP under Criteria A–C and has not been evaluated under Criterion D. The HPO concurred with this recommendation and found this resource to be not eligible for the NRHP.

9. Office Building  
600 W. Cabarrus Street, Raleigh  
PIN: 1703475257





The primary resource at 600 W. Cabarrus Street is a one-story office building constructed around 1962. It has a masonry structural system that is clad in a brick, stretcher-bond veneer. The building is covered by very low-pitched shed roof that slopes to the north and is lined with metal coping. The primary entrance is located on the south elevation and is accessed by a set of brick steps. Other fenestrations include narrow ribbon windows on the south elevation and single-light, fixed windows on the north, east, and west elevations.

Though it is in good condition, this resource does not exhibit high artistic value as the work of a master, nor is it an outstanding example of a particular architectural style. Modifications over time have somewhat diminished its historic integrity. It was recommended not eligible for individual listing on the NRHP under Criteria A–C and has not been evaluated under Criterion D. The HPO concurred with this recommendation and found this resource to be not eligible for the NRHP.

10. Warehouse  
414 Dupont Circle, Raleigh  
PIN: 2928316376



Built around 1949, the primary resource at 414 Dupont Circle is a one-story commercial building constructed around 1949. This resource has a cinderblock structural system that is clad in a brick, stretcher-bond veneer on the east elevation. The building is covered by a very low-pitched shed roof that is lined with a brick parapet on the southern end of the east elevation and a metal coping near the middle of the building. There are several entrances on the east elevation including replacement, six-panel, metal doors. Other fenestrations include metal, overhead garage doors and replacement, metal-frame windows. Many of the original window openings have been filled in with brick. The northern end of the building is likely a later addition. It has a cinderblock structural system that is covered by a shed roof. This part of the building features two metal doors, paired vinyl windows, and an overhead, metal garage door.

Although this resource is in fair condition, modifications over time have somewhat diminished its historic integrity. This resource does not exhibit high artistic value as the work of a master, nor is it an outstanding example of a particular architectural style. It was recommended not eligible for individual listing on the NRHP under Criteria A–C and has not been evaluated under Criterion D. The HPO concurred with this recommendation and found this resource to be not eligible for the NRHP.

11. Warehouse  
411 W. Morgan Street/117 S. West Street, Raleigh  
PIN: 1703580892



The primary resource at 411 W. Morgan Street/117 S. West Street is a one-story warehouse built around 1950. The building's foundation is not visible; however, the structural system is laid in a brick, five-to-one American bond. It is covered by a very low-pitched arched roof that is lined with a very low, brick parapet; a stepped parapet is located near the middle of the building on the west elevation. There are several entrances on this building located on the east, south, and north elevations. A majority of the windows are original, louvered windows set within a metal frame. An unloading dock is situated on the north elevation.

Though it is in good condition, this resource does not exhibit high artistic value as the work of a master, nor is it an outstanding example of a particular architectural style. Modifications over time have somewhat diminished its historic integrity. It was recommended not eligible for individual listing on the NRHP under Criteria A–C and has not been evaluated under Criterion D. The HPO did not concur with this recommendation and requested additional study of these resources.

12. Commercial Building  
100 S. Harrington Street, Raleigh  
PIN: 1703581892



The primary resource at 100 S. Harrington Street is a one-story commercial building constructed around 1952. The building's foundation is not visible; however, the structural system is clad in a brick, stretcher-bond veneer. A decorative stone veneer is on the easternmost side of the north

elevation. The resource is covered by either a flat or a low-pitched roof. The primary entrance, a contemporary plate-glass door within a metal frame, is off-centered on the north elevation. Other fenestrations include plate-glass windows. An awning covered in standing-seam metal projects from the north elevation.

Though it is in good condition, this resource does not exhibit high artistic value as the work of a master, nor is it an outstanding example of a particular architectural style. Modifications over time have somewhat diminished its historic integrity. It was recommended not eligible for individual listing on the NRHP under Criteria A–C and has not been evaluated under Criterion D. The HPO concurred with this recommendation and found this resource to be not eligible for the NRHP.

**APPENDIX B: LIST OF PROPERTIES  
SURVEYED AS PART OF A DISTRICT**

## Resources Surveyed and Evaluated as Part of a District

The following 38 resources are located within the boundaries of historic districts: Boylan Heights Historic District, Depot Historic District, S. Boylan Avenue Historic District, and the Old Fourth Ward Historic District. During the current survey, these properties were recorded as part of the historic district instead of as individual resources during the December 2012 reconnaissance survey.

Table 2: Resources Evaluated as a Part of a District.

| Address/Name             | Date | Associated Historic District                         |
|--------------------------|------|--|
| 106 Dupont Circle        | 1915 | Boylan Heights Historic District                     |
| 305 Kinsey Street        | 1915 | Boylan Heights Historic District                     |
| 307 Kinsey Street        | 1918 | Boylan Heights Historic District                     |
| 311 Kinsey Street        | 1915 | Boylan Heights Historic District                     |
| 315 Kinsey Street        | 1917 | Boylan Heights Historic District                     |
| 310 S. Harrington Street | 1930 | Depot Historic District                              |
| 318 S. Harrington Street | 1930 | Depot Historic District                              |
| 121 S. Boylan Avenue     | 1905 | S. Boylan Avenue Historic District                   |
| 131 S. Boylan Avenue     | 1901 | S. Boylan Avenue Historic District                   |
| 514 S. Saunders Street   | 1925 | Old Fourth Ward Historic District Proposed Expansion |
| 510 S. Saunders Street   | 1915 | Old Fourth Ward Historic District Proposed Expansion |
| 502 W. Lenoir Street     | 1948 | Old Fourth Ward Historic District Proposed Expansion |
| 504 S. Saunders Street   | 1950 | Old Fourth Ward Historic District Proposed Expansion |
| 505 S. Saunders Street   | 1915 | Old Fourth Ward Historic District Proposed Expansion |
| 506 S. Saunders Street   | 1920 | Old Fourth Ward Historic District Proposed Expansion |
| 508 S. Saunders Street   | 1910 | Old Fourth Ward Historic District Proposed Expansion |
| 509 S. Saunders Street   | 1925 | Old Fourth Ward Historic District Proposed Expansion |
| 511 S. Saunders Street   | 1910 | Old Fourth Ward Historic District Proposed Expansion |
| 512 S. Saunders Street   | 1920 | Old Fourth Ward Historic District Proposed Expansion |
| 513 S. Saunders Street   | 1910 | Old Fourth Ward Historic District Proposed Expansion |

| <b>Address/Name</b>    | <b>Date</b> | <b>Associated Historic District</b>                  |
|------------------------|-------------|--|
| 515 S. Saunders Street | 1925        | Old Fourth Ward Historic District Proposed Expansion |
| 515 W. Lenoir Street   | 1915        | Old Fourth Ward Historic District Proposed Expansion |
| 516 S. Saunders Street | 1945        | Old Fourth Ward Historic District Proposed Expansion |
| 516 W. Lenoir Street   | 1915        | Old Fourth Ward Historic District Proposed Expansion |
| 517 S. West Street     | 1930        | Old Fourth Ward Historic District Proposed Expansion |
| 518 S Saunders Street  | 1920        | Old Fourth Ward Historic District Proposed Expansion |
| 518 W. Lenoir Street   | 1915        | Old Fourth Ward Historic District Proposed Expansion |
| 519 W. Lenoir Street   | 1915        | Old Fourth Ward Historic District Proposed Expansion |
| 520 S. Saunders Street | 1950        | Old Fourth Ward Historic District Proposed Expansion |
| 520 W. Lenoir Street   | 1915        | Old Fourth Ward Historic District Proposed Expansion |
| 521 W. Cabarrus        | 1920        | Old Fourth Ward Historic District Proposed Expansion |
| 522 W. Lenoir Street   | 1915        | Old Fourth Ward Historic District Proposed Expansion |
| 523 S. West Street     | 1930        | Old Fourth Ward Historic District Proposed Expansion |
| 524 W. Lenoir Street   | 1915        | Old Fourth Ward Historic District Proposed Expansion |
| 526 W. Lenoir Street   | 1935        | Old Fourth Ward Historic District Proposed Expansion |
| 603 W. Cabarrus        | 1925        | Old Fourth Ward Historic District Proposed Expansion |
| 606 W. Lenoir Street   | 1930        | Old Fourth Ward Historic District Proposed Expansion |
| 607 W. Cabarrus        | 1925        | Old Fourth Ward Historic District Proposed Expansion |



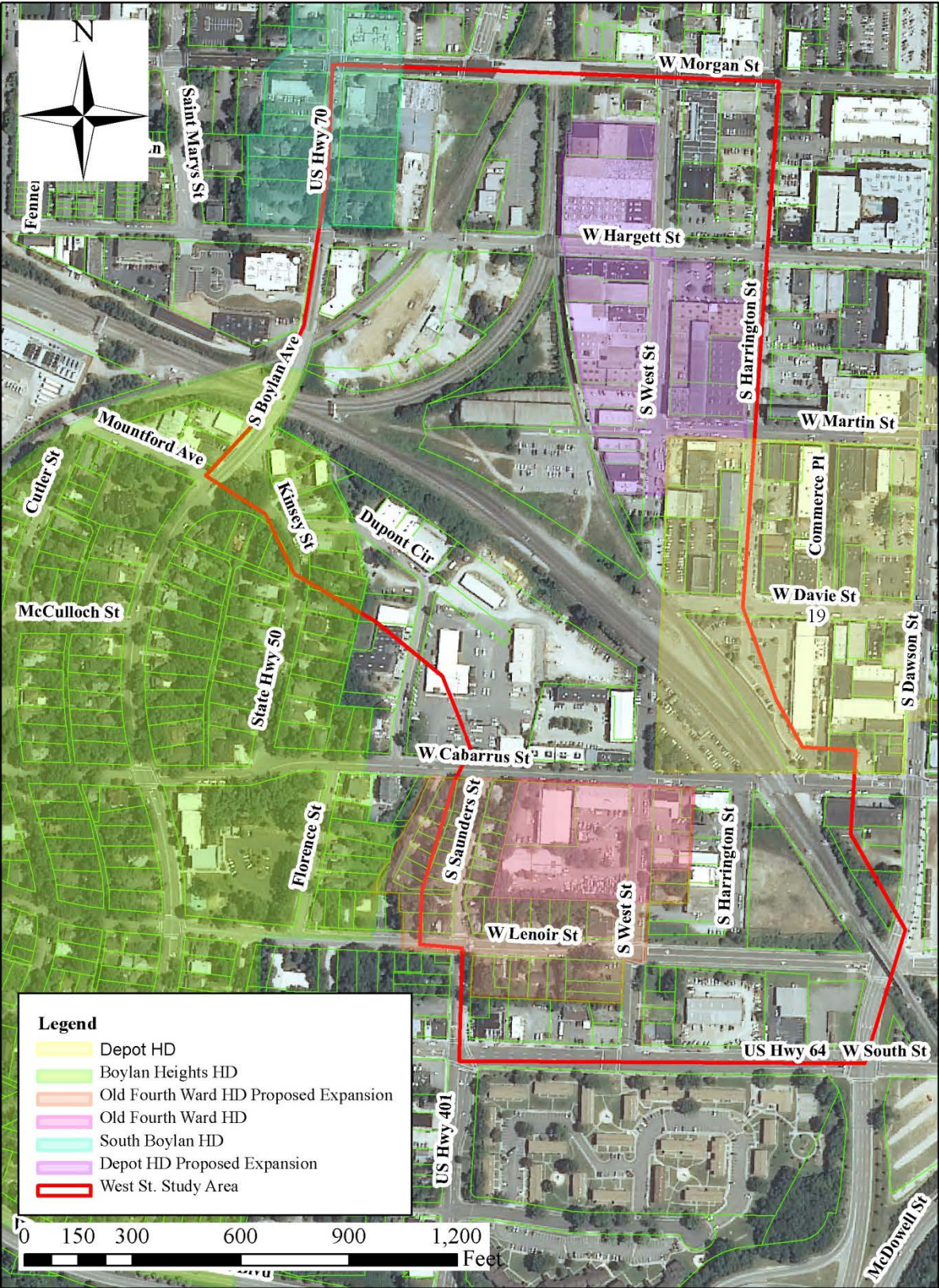


Figure 19: Areas Surveyed as Part of a Historic District.

**APPENDIX C: LIST OF PROPERTIES  
NOT EVALUATED DURING THIS INVESTIGATION**

## Resources Not Evaluated During this Investigation

Within the West Street Extension Project APE, there are 68 resources over the age of 50 that were not recorded as part of this project. Almost half (n=30) of those were surveyed in 2012 by Mattson, Alexander Associates, Inc. (MAA 2013). After consulting with the HPO, it was determined that the following properties documented by MAA did not require additional survey at this time. A high percentage of the buildings are located within the boundaries of historic districts, such as the Depot Historic District, Depot Historic District Expansion, and Boylan Heights Historic District.

Table 3: Resources Documented by MAA and Excluded from the Current Study.

| <b>SURVEY #</b> | <b>HPO #</b> | <b>Address/Name</b>            | <b>Date</b> | <b>MAA Recommendation</b>         |
|-----------------|--------------|--------------------------------|-------------|-----------------------------------|
| 8               | WA 3008      | 501 W. Morgan Street           | 1930        | No Further Study                  |
| 9               |              | 110 S. West Street             | 1939        | Depot Historic District Expansion |
| 10              |              | Peden Steel Works              | 1940        | Depot Historic District Expansion |
| 11              | WA 3199      | 200 S. West Street             | 1920        | Depot Historic District Expansion |
| 12              |              | 206 S. West Street             | 1922        | Depot Historic District Expansion |
| 13              |              | 210 S. West Street             | 1910        | Depot Historic District Expansion |
| 14              |              | 230 S. West Street             | 1940        | Depot Historic District           |
| 15              |              | 302 S. West Street             | 1914        | Depot Historic District Expansion |
| 16              |              | 304 S. West Street             | 1914        | Depot Historic District Expansion |
| 17              |              | 310/323 S. West Street         | 1914        | Depot Historic District Expansion |
| 19              | WA2574       | Southern Railway Freight Depot | 1912        | Depot Historic District           |
| 20              |              | 319 W. West Street             | 1920        | Depot Historic District           |
| 21              |              | 320 S. Harrington Street       | 1915        | Depot Historic District           |
| 22              | WA 2591      | 324 S. Harrington Street       | 1925        | Depot Historic District           |
| 23              | WA 2576      | 406 W. Davie Street            | 1915        | Depot Historic District           |

| <b>SURVEY #</b> | <b>HPO #</b> | <b>Address/Name</b>  | <b>Date</b> | <b>MAA Recommendation</b>            |
|-----------------|--------------|--|-------------|--------------------------------------|
| 24              | WA 2569      | 316 W. Cabarrus Street   | 1950        | Depot Historic District              |
| 25              | WA 2570      | 320 W. Cabarrus Street,<br>Southern Railway<br>Passenger Station | 1949        | Depot Historic District              |
| 26              |              | 301 W. Cabarrus Street   | 1957        | No Further Study                     |
| 27              |              | 401 W. Cabarrus  | 1930        | No Further Study                     |
| 28              |              | 400 S. West Street   | 1956        | No Further Study                     |
| 30              |              | 505 S. West Street   | 1946        | Old Fourth Ward                      |
| 31              |              | 518 S. West Street   | 1946        | Old Fourth Ward                      |
| 32              |              | 324 Dupont Circle  | 1945        | No Further Study                     |
| 33              |              | 328 Dupont Circle  | 1950        | No Further Study                     |
| 34              |              | 330 Dupont Circle  | 1950        | No Further Study                     |
| 36              |              | 301 Kinsey Street  | 1920        | Boylan Heights Historic<br>District  |
| 37              |              | 303 Kinsey Street  | 1925        | Boylan Heights Historic<br>District  |
| 39              |              | 629 W. Hargett Street  | 1950        | No Further Study                     |
| 40              |              | 615 W. Hargett Street  | 1940        | No Further Study                     |
| 41              |              | 607 W. Morgan Street   | 1937        | No Further Study                     |
|                 |              | 409 W. Martin Street   | 1927        | Depot Historic District              |
|                 |              | 223 W. Street/410 W.<br>Martin/218 S. Harrington                 | 1952        | Depot Historic District<br>Expansion |





**APPENDIX D: CHAIN OF TITLE FOR STUDY PROPERTIES**



**12-041: Chain of Title 411 W. Morgan Street, Raleigh**

| <b>GPIN/ Tax Parcel No.</b> | <b>INSTR. TYPE</b> | <b>BK</b> | <b>PG</b> | <b>DATE</b> | <b>GRANTOR</b>                     | <b>GRANTEE</b>                   | <b>AC</b> | <b>NOTES / COMMENTS</b>  |
|-----------------------------|--------------------|-----------|-----------|-------------|------------------------------------|----------------------------------|-----------|--|
| 1703580892<br>000/ 0013944  | DEED               | 12540     | 2159      | 4/27/2007   | Coggins Construction Co            | 411 W Morgan Street LLC (leasee) | 0.5       | Exhibit A indicates that the parcel is 105 ft x 210 ft in size... surveyed by Fred W. Habel dated May 1950... property conveyed to Lyon Equipment Co, succeeded by Coggins Construction Co, by deed on July 31, 1973 (Bk 2184:29).   |
|                             | DEED               | 8221      | 86        | 10/13/1998  | Coggins Construction Co            | Jullians Billiard Café (leasee)  | 0.5       | Exhibit A indicates that the parcel is 105 ft x 210 ft in size... surveyed by Fred W. Mabel in May 1950 after prop was conveyed to John W. & Thelma G. Coffey (Feb 2, 1950 in Bk 1038:413).  |
|                             | Articles of Merger | 2641      | 190       | 12/31/1981  | Lyon Equipment Co                  | Coggins Construction Company     | 0.5       | Merger became effective July 31, 1978.   |
|                             | DEED               | 2184      | 29        | 7/31/1973   | Edward I. & Patsy V. Cole          | Lyon Equipment Co                |           | Same descriptions as above...property conveyed to John N. Coffey (& Thelma G) on Feb 1, 1950 (Bk 1038:413). Together with all interest described in deed from The Capital Coca-Cola Bottling Co Inc to John Coffey on Jan 30,1960 (Bk 1408:316).   |
|                             | DEED               | 2098      | 219       | 8/31/1972   | John N. & Thelma G. Coffey         | Edward I. & Patsy V. Cole        |           | For \$100 and other considerations... same description & references as previous...at that time it was leased to Fairmont Foods Company (good until Apr 15, 1973).  |
|                             | DEED               | 1408      | 316       | 1/30/1960   | The Capital Coca-Cola Bottling Co  | John N. Coffey                   |           | Appears to convey part of 117 S. West St...purchased by the Coca-Cola Bottling Co on Dec 6, 1947 (Bk 984:236). Notes shared boundary wall.   |
|                             | DEED               | 1038      | 413       | 2/1/1950    | L Conway & Louise Burke Murchinson | John N. & Thelma G. Coffey       |           | For \$100 and other considerations. Same physical description... acknowledges survey by Harry Tucker dated Aug 1924, is referenced to the City Monuments at the intersection of Hargett and West Street, and the intersect of Hargett and Dawson Streets, being the same property conveyed to The School Committee of Raleigh Township by W. K. Davis et al. on July 3, 1896 (Bk 140:89), and being same prop conveyed by William Bailey Jones, Trustee to W. S. Murchinson (Bk 608:45). |

**12-041: Chain of Title 411 W. Morgan Street, Raleigh**

|  |      |     |     |            |   |                                      |   |
|--|------|-----|-----|------------|---|--------------------------------------|---|
|  | DEED | 608 | 45  | 11/7/1930  | William Bailey Jones, TR                          | W. S. Murchinson                     | Whereas, on Oct 25, 1925, WS & Alma Murchinson, and CA & Elgettie Richardson, executed a certain deed of trust with WB Jones (Bk 461:549 owing \$20,000) and they defaulted. After advertizing sale on Oct 27 1930, W. S. Murchinson became highest bidder (\$23,000) for 105 x 210 ft. lot in Raleigh... survey by Tucker. Being same property sold to School Committee in 1896 (Bk 140:89). |
|  | DOT  | 461 | 549 | 10/26/1925 | Murchinson & Richardson                           | W B Jones                            | Being the same property conveyed to W S Murchinson by W K Davis and others on July 3, 1896 (Bk 140:89).   |
|  | DEED | 140 | 89  | 7/10/1896  | W. K. & Mary A. Davis and W. P. & E. Millie Jones | School Committee of Raleigh Township | In consideration of \$2600... parcel of about 0.5 acres, same lot conveyed to K Jones by C Jones (Bk 28:96) and afterwards devised by K Jones to his wife, Mary W., and Mary W Jones to Mary A. Davis and others...   |

12-041: Chain of Title 117-121 S. West Street, Raleigh

| GPIN/ Tax Parcel No.          | INSTR. TYPE | DB   | PG  | DATE       | GRANTOR                          | GRANTEE   | AC                     | NOTES / COMMENTS  |
|-------------------------------|-------------|------|-----|------------|----------------------------------|---|------------------------|---|
| 1703580676<br>000/<br>0077086 | DEED        | 6865 | 91  | 2/28/1996  | Erickson Photography, Inc.       | Empire Properties LLC (now 133 Fayetteville St, Ste 600, Raleigh... historically 117 S West St) | 0.37                   | Two tracts... The first is property of study (105 x 105 ft) and contains a one-story brick building shown on survey dated Oct 14, 1987 by Chasak for Erickson Photo. Being the same land conveyed by Alread Williams to grantor (Bk 4182:263) & subject to rights/party agmnt estab by Capital Coca-Cola Bottling Co. to Gulf Oil Co. (see Bk 1301:326) & in deed from Gulf Oil Co to B. Grimes Williams & Alfred Williams (Bk 1301:313). Second Tract extends from SE corner of Tract 1 and along S. Harrington...subject to alleyway on S side of lot & N side of what was formerly FJ Hollowoy's lot as created in deed Mar 15, 1901 (Bk 166:172). |
|                               | DEED        | 4182 | 263 | 1/6/1988   | Alfred Williams (unmarried)      | Erickson Photography, Inc.  |                        | Property on E side of S. West St... in survey by Fred W. Habel dated Nov. 18, 1957.   |
|                               | DEED        | 1689 | 408 | 12/13/1965 | B. Grimes & Margaret O. Williams | Alfred Williams (brother)   | 117                    | For \$10 and other considerations... 4 tracts. The lot at 117 S. West Street, two others on Hillsborough St, and one at Hargett and Snow.   |
|                               | DEED        | 1301 | 313 | 12/31/1957 | Gulf Oil Corporation             | B. Grimes & Alfred Williams   | 117-121 S. West Street | For \$100 and other considerations... see survey of Habel... 105 ft sq. lot with 13" party wall along southern boundary... conveyed to Gulf Oil Corporation by Capitol Coca-Cola Bottling Co.   |
|                               | DEED        |      |     |            | Coca-Cola Bottling Co.           | Gulf Oil Corporation  |                        |   |
|                               | DEED        | 1002 | 589 | 9/1/1948   | Wade H. Riggan (widower) et al.  | Capital Coca-Cola Bottling Company  | 121 S. West Street     | For \$100 and other considerations... the lot and improvements... beginning at the NE corner of the intersect of Hargett and West Streets, then running east 52' and north 105' to the lot sold by D.M. Carter & Commissioners to the State of NC (Lot No. 2), then with this lot west to West Street.  |



**12-041: Chain of Title 117–121 S. West Street, Raleigh**

|  |      |     |     |           |                             |                             |  |  |
|--|------|-----|-----|-----------|-----------------------------|-----------------------------|--|--|
|  | DEED | 984 | 236 | 12/6/1947 | L. C. & Louise B. Muchinson | Coca-Cola Bottling Co.      |  | For \$100 + other considerations... tract begin at point on E side West St/SW cor of Wiley Hotel prop, 243' south of Morgan St, running S 105' along S. West Street then 105' east to cor of Riggan & Watson... being all that property conveyed by H.D. Bunch & wife on Apr 23, 1946 to Murchin (Bk 940 p. 563).  |
|  | DEED | 940 | 563 | 4/23/1946 | H. D. & Margaret H. Bunch   | L.C. & Louise B. Murchinson |  | for \$100 and other considerations... two tracts. One being part of lot no. 154 in Shaffer's map of the City, bordering John M. Monie (north), and W. Riggan/Fred Watson (south), Methodist Ch lot (east), West St (west). Roughly 47' by 108' in size. This lot part of land J.N. Holding sold to Monie (Bk 143:421) and same Monie sold to Bunch in 1911 (Bk 250:244). Second tract, also part of lot 154... lot on north side of tract one (58' x 107')... to south line of Wiley graded School... being same land conveyed to Monie from trustees. |

12-041: Chain of Title 520 W. South Street, Raleigh

| GPIN/<br>Tax<br>Parcel<br>No.  | INSTR.<br>TYPE | DB    | PG   | DATE      | GRANTOR  | GRANTEE   | AC              | NOTES / COMMENTS   |
|--------------------------------|----------------|-------|------|-----------|--|---|-----------------|--|
| 17034653<br>11 000/<br>0079391 | DEED           | 14601 | 1591 | 1/3/2012  | Phyllis M. Joyner et al. for<br>the Barney G. Joyner<br>Family Trust (815 New<br>Bern Ave) | 520 W South Street<br>LLC (815 New Bern<br>Ave) | 0.22            | Two tracts conveyed here, same description as previous deed.   |
|                                | DEED           | 4554  | 121  | 8/28/1989 | M. W. Woodard (widower)  | Barney G. Joyner (815<br>New Bern Ave)          |                 | Two tracts: Tract 1 is a 52.5' x 115' lot along east side of 520 & and is visible on Rex Hospital Property plat (Bk 125:99). Tract 2 is where the station is now located, known as Lot 9 of Block 4 in the same plat and being 52.5' x 115' in size. Acquired in 1939 (Bk 810 440 and 448).  |
|                                | DEED           | 810   | 440  | 7/25/1939 | Annie E. Crawley (widow<br>of A. W. Crawley)   | M. W. Woodard &<br>wife, Mary Frances           |                 | For \$100 and other considerations, lot 52.5 feet east of NE corner of Saunders & South St... being same land conveyed to D. S. & Mary Crawley on May 23, 1936.  |
|                                | DEED           | 810   | 448  | 7/25/1939 | John H. & Mabel Y.<br>Andrews  | M. W. Woodard &<br>wife, Mary Frances           | Lot 9,<br>Blk 4 | For \$100 and other considerations, Lot 9 of Blk 4 at NE corner of Saunders & South St... being land conveyed by Andrew Brown (A. B.) Vincent to Fred Worth on Jan 23, 1922 (Bk 381:414) and from M. S. Brown Sheperd (Atty) to Hal V. Worth (Bk 432:434). Same tract Hal Worth purchased from Cary Development Co on Mar 25, 1926 (Bk 486:342). See also deed Bk 614:548. |
|                                | DEED           | 712   | 298  | 5/23/1936 | D. S. & Mary Crawley, of<br>Chesterfield Co, NC  | Annie E. Crawley                                |                 | For \$10 and other considerations... eastern half of 520 W. South Street. Being the same property conveyed to Laura and Miner Milliken in 1903 (Bk 187:67).  |
|                                | DEED           | 486   | 342  | 3/25/1926 | Hal V. & Marguerite Worth  | Cary Development Co.                            | Lot 9,<br>Blk 4 | For \$100 and other considerations... being land conveyed by A.B. Vincent to Fred Worth on Jan 23, 1922 (Bk 381:414). Ref also made to Mar 10, 1924 deed from S. Brown Sheperd atty to Hal V. Worth (Bk 432:434). Deed of Trust made between Worth & Shepherd for \$2500 on Oct 31, 1925 (Bk 472:604), and payment to be assumed by grantee.                               |
|                                | DEED           | 432   | 434  | 3/10/1924 | S. Brown Shepherd (Atty)   | Hal V. Worth                                    | Lot 9,<br>Blk 4 | Whereas Fred & Martha Worth (Jan 13, 1923, Bk 404:417) gave this land to Brown as collateral for a debt, and defaulted on payment. Public sale on Feb 28, 1924 and land sold to Hal V. Worth for \$1,400. Whereas Hal has paid in full... now conveys Lot 9 in Blk 4. Being land sold by A.B. Vincent to Fred Worth (Bk 381:414).  |

**12-041: Chain of Title 520 W. South Street, Raleigh**

|  |      |     |     |            |                                 |                        |  |  |
|--|------|-----|-----|------------|---------------------------------|------------------------|--|--|
|  | DEED | 381 | 414 | 1/23/1922  | A. B. Vincent and Cora P., wife | Fred Worth             |  | For \$100 and other considerations... land at NE corner of South & Saunders adjoining Miner Miligan & Poe... being same lot conveyed to Vincent by trustees of Rex Hospital (Bk 129:16).   |
|  | DEED | 187 | 67  | 10/19/1903 | A. B. Vincent and Cora P., wife | Laura & Miner Milliken |  | For \$500... lot located 52.5 ft east of Saunders & South St intersection...adj lands of Vincent & Hasty Poe.  |
|  | DEED | 127 | 16  | 3/27/1893  | Trustees of Rex Hospital        | A. B. Vincent          |  | For \$388... lots adj land of Rex Hospital, W. Whitaker and others... First two lots at NW cor of West & South Streets (lot 3 & 4 of Blk 4). Second two lots, begin at NE cor of South & Saunders St extended... (lots * and 9 in Blk 4). Same land sold at public auction March 1893 in accordance with Superior Court of Wake Co. judgment, Oct term of 1892 (R. H. Battle, President of Rex Hospital). See also Vincent to Rex Hospital mortgage (Bk 127:17). |

